

6.20 Special Policy Area 20 – Bradford GO Major Transit Station Area

Special Policy Area 20, as shown on Schedules B1-A of this Official Plan encompasses a total area of approximately 62 hectares located within an 800-metre (10-minute walking) radius of the Bradford GO Station, at the intersection of Dissette, Holland, and Bridge Streets. As a designated Major Transit Station Area (MTSA), this location is planned to accommodate the largest share of the Town's growth to 2051, with a forecast of 12,800 people and jobs. The MTSA supports and implements the policy directions of the *Provincial Planning Statement* and County of Simcoe Official Plan by targeting the achievement of a minimum density target of 150 residents and jobs per hectare.

The MTSA is envisioned as a compact, transit-supportive, and pedestrian-oriented community that will support and integrate a diverse mix of residential, commercial, institutional and recreational uses, supported by active and accessible transit connections. This area will promote reduced automobile reliance and support the creation of complete and livable communities.

While the MTSA will accommodate the highest intensity of growth in the Town, development will not be uniform throughout. To recognize and reflect the distinct characteristics, opportunities and development potential of different parts of the MTSA, the area is structured into four distinct corridors:

- Bridge Street Corridor;
- Dissette Street Corridor;
- Holland Street Corridor; and,
- Residential Edges (north and south)

While each corridor will retain its distinct identity, shaped by context-sensitive policies on land use, built form, and density; all corridors will collectively contribute to a cohesive vision: a connected, vibrant, and complete community that balances small-town charm with urban vitality. This planning framework will guide development through to 2051, positioning the Bradford GO MTSA as both a destination and a point of origin

6.20.1 Vision

The Major Transit Station Area (MTSA) will evolve into a vibrant, transit-oriented community where Bradford's small-town charm is thoughtfully and harmoniously integrated with compact, urban development. Anchored by the Bradford GO Station, the MTSA will become a dynamic

hub offering a range of housing options, job opportunities, and access to retail, cultural, and recreational amenities—all within a pedestrian-focused environment.

As the area grows, it will achieve higher densities and provide a range of housing options while preserving and celebrating Bradford's cultural heritage and historic character. Development will be shaped by strong urban design principles, including a network of pedestrian connections, multimodal transportation options, public spaces, and Privately Owned Public Spaces (POPs). Growth will be seamlessly integrated with surrounding agricultural and residential areas to ensure compatibility and cohesion.

The MTSA will serve as both a destination and a gateway—connecting residents to regional opportunities while creating a distinct sense of place that honours Bradford's past and embraces its future. Through thoughtful design and compact development, it will foster a human-scaled urban environment that supports a vibrant pedestrian realm—inviting people to walk, bike, work, gather, and play in spaces that feel connected, comfortable, and inclusive.

6.20.2 Goals and Objectives

As the Bradford Major Transit Station Area evolves, the Town will be guided by the following objectives:

- a) Create a distinctive community identity that accommodates growth while preserving and celebrating the Town's heritage.
- b) Establish a vibrant mixed-use community with an active streetscape, engaging public realm and a welcoming eastern gateway along the Bridge Street Corridor.
- c) Develop compact, complete communities with diverse and affordable housing options and job opportunities.
- d) Develop a connected network of public spaces, essential services, and multimodal transportation options that prioritize pedestrians and cyclists with seamless access to the GO Station.
- e) Advance sustainability and climate resilience through compact urban form, green infrastructure and spaces.
- f) Accommodate growth by achieving the provincially-mandated minimum density target of 150 people and jobs per hectare.

6.20.3 Major Transit Station Area Corridors Policies

The distribution of population and employment growth within the Major Transit Station Area (MTSA) is planned through four distinct corridor categories, as identified on Schedule B1-A of this Official Plan. Each corridor reflects unique characteristics related to context, land use, and built form, and is intended to accommodate varying levels of residential development, density, and growth in a manner that is tailored to its character and constraints, as generally illustrated in Figure 1:

MTSA Corridor	Units	UPH*	Residents	Commercial Area	Jobs
<i>Dissette Street</i>	3,990	300	7,600	5,900 sq. m.	121
<i>Holland Street</i>	860	200	1,500	6,700 sq. m.	107
<i>Residential Edges (north & south)</i>	360	40	1,000	0	0
<i>Bridge Street</i>	0	0	0	3,200 sq. m.	58
<i>Total Net Growth</i>	5,210	-	10,100	15,800 sq. m.	286

Figure 1 – Net People and Jobs estimates per corridor to 2051

*UPH- stands for Units per Net Hectare

The Dissette Street Corridor is expected to experience the highest level of growth, followed by the Holland Street Corridor. The residential edges of the MTSA are comprised of established neighbourhoods and are therefore not anticipated to accommodate significant additional growth over. Due to natural hazard constraints and flood risk, residential development is not permitted within the Bridge Street Corridor until the Town has implemented the measures outlined in policy 6.20.4.2 a) vii). In the interim, commercial development may proceed in select southwestern portions of the corridor, subject to the requirements set out in Section 6.20.5. Figure 2 illustrates the projected contribution of each corridor to the overall growth of the Major Transit Station Area (MTSA), which is anticipated to accommodate approximately 10,100 additional residents and 290 additional jobs by 2051:

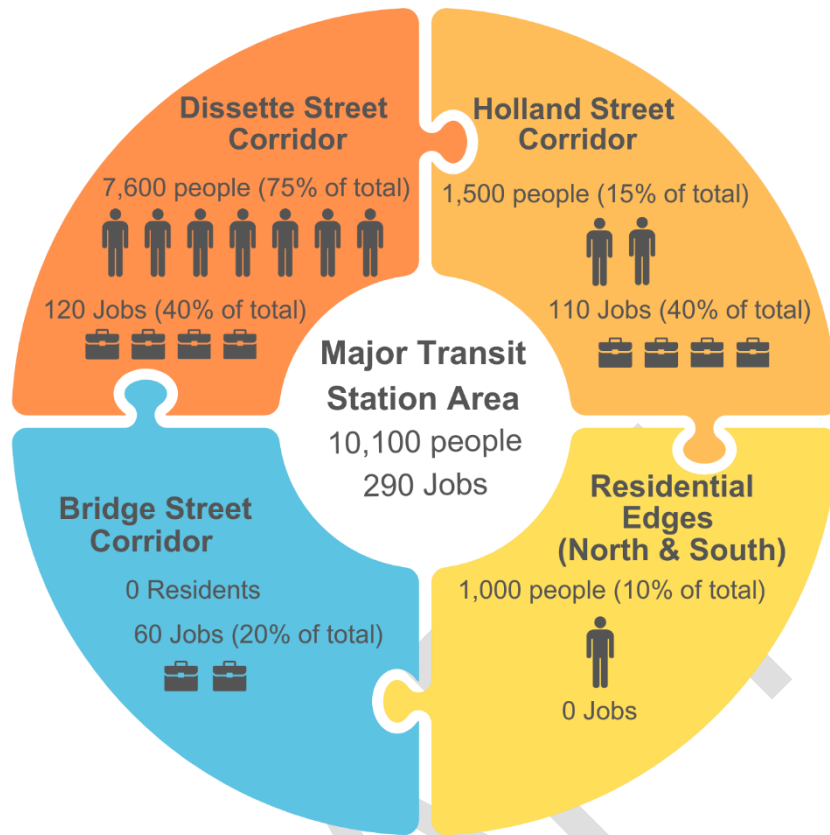


Figure 2 - The estimated proportion of people and jobs to be added per corridor by 2051

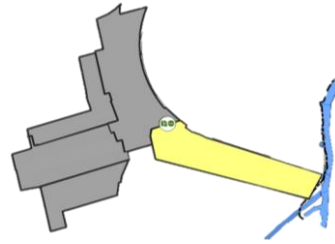
The policies in this Section are organized according to the corridor categories identified on Schedule B1-A, as follows:

- Policies applicable to the Bridge Street Corridor where commercial development is permitted;
- Policies for portions of the Bridge Street Corridor and other corridors located within the floodplain;
- Policies for the Holland Street Corridor;
- Policies for the Dissette Street Corridor; and
- General area-wide policies applicable to the entire Major Transit Station Area.

These policies shall be read in conjunction with all other applicable policies of this Plan. In the event of a conflict, the policies contained in this Section shall prevail.

6.20.4 Bridge Street Corridor Policies

The Bridge Street Corridor forms the eastern portion of the Town's Major Transit Station Area (MTSA), encompassing the Bradford GO Station, as shown on Schedule B1-A. Located just west of the Holland River, this corridor has long been a centre of mobility, commerce, and regional connectivity.



Its historical significance is rooted in its proximity to the Holland River—a vital extension of the Carrying Place Trail, a major Indigenous trade and travel route used by the Wendat (Huron), Haudenosaunee (including the Onondowahgah, or Seneca), Anishinaabe, and Mississaugas. The trail connected Lake Ontario to Lake Simcoe and beyond, with its most documented route passing through the west Holland River adjacent to what is now the Bridge Street Corridor. Travellers journeyed north from the shores of Lake Ontario via the Humber River valley, crossed the Holland Marsh, and launched canoes onto the Holland River en route to Lake Simcoe, Georgian Bay, or westward to Michilimackinac—an important Indigenous trading centre at the confluence of Lakes Huron and Michigan. Along this extensive travel network, goods, knowledge, and culture were exchanged, making it one of the busiest trading routes in Southern Ontario. Through this connection, the Bridge Street Corridor became part of an enduring system of movement and exchange, establishing its legacy as a place of cultural, economic, and regional significance.

With the arrival of European settlers in the 18th and 19th centuries, the area remained a transportation hub, bolstered by the development of roads and the Northern Railway. The opening of Bradford's first train station in 1853—on the site now occupied by the Bradford GO Station—strengthened the corridor's role as a conduit for agricultural goods from surrounding farms and the Holland Marsh to expanding urban markets.

This legacy of connectivity informs the vision for the corridor's future: to once again function as a vital connector—linking people to regional opportunities while fostering a distinct sense of place that honours the past and supports a vibrant, inclusive future. Anchored by the Bradford GO Station, the Bridge Street Corridor is expected to accommodate approximately 60 new jobs by 2051. Through compact, human-scaled development and thoughtful urban design, the corridor will support a dynamic, transit-oriented environment with a vibrant pedestrian realm—inviting people to walk, bike, work, and gather in spaces that are accessible, comfortable, and connected. As it evolves, the corridor will continue to reflect its historic role as a place of movement, interaction, and cultural exchange.

6.20.4.1 Bridge Street Corridor Commercial Areas

In recognition of the Bridge Street Corridor's legacy and longstanding cultural and historical role as a key route for facilitating commerce and connectivity to regional and urban markets, commercial uses are permitted in select areas, subject to the policies outlined below:

- a) Notwithstanding the natural hazard and floodplain policies of this Official Plan, the following policies apply to the southwestern portion of lands in the Bridge Street Corridor, as identified in Figure 3 below. This area is located approximately 320 metres west from the Holland River shoreline, adjacent to Bridge Street:
 - i) A full range of retail, service commercial, recreational, and small-scale office uses may be permitted, subject to a maximum development depth of 67 metres from the lot line fronting Bridge Street and shall contribute to the projected number of jobs for the Major Transit Station Area.
 - ii) All redevelopment shall be floodproofed to an elevation determined by, and to the satisfaction of, the Lake Simcoe Region Conservation Authority.
 - iii) A minimum 15-metre landscaped buffer shall be provided adjacent to the drainage ditch generally located along the southern boundary of the Major Transit Station Area. The final width and design of the buffer shall be subject to the satisfaction of the Lake Simcoe Region Conservation Authority.
 - iv) Only surface parking at grade shall be permitted in the area located between the 67-metre maximum development depth and the 15-metre landscaped buffer to the south. The design and placement of such parking shall be to the satisfaction of the Lake Simcoe Region Conservation Authority.
 - v) Residential uses shall be prohibited on these lands.

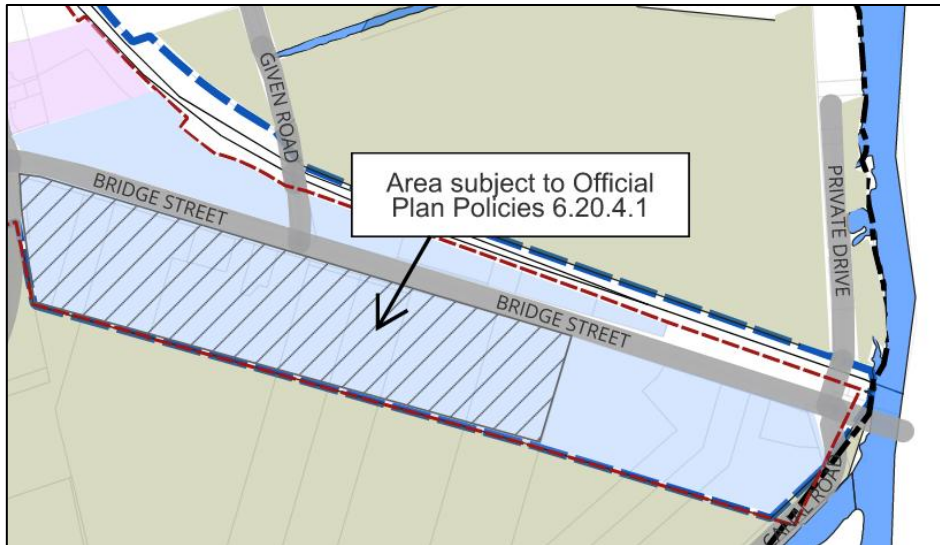


Figure 3 - Commercially Permitted Uses areas on Bridge Street

- b) For all other lands within the Bridge Street Corridor that are not subject to the permissions outlined in Section 6.20.4.1, the policies of Section 6.20.4.2 and 6.20.4.3 shall apply.

6.20.4.2 Floodplain Overlay

As shown on Schedule B1-A, the entirety of the Bridge Street Corridor—along with portions of the Dissette Street Corridor and the South Residential Edge Corridor—is located within a regulated floodplain and is subject to natural hazard policies. The flood susceptibility of these areas is intrinsically linked to the environmental history of the surrounding Holland Marsh, which has shaped the low-lying topography and ongoing flood risk.

Historically, the land surrounding Bridge Street was part of a vast wetland basin—an estuary of prehistoric Lake Algonquin near the end of the last Ice Age. As the lake receded toward its present-day boundaries at Lakes Simcoe and Huron, the area remained saturated, forming a rich organic wetland. This landscape is now known as the Holland Marsh—a unique ecological area historically used by Indigenous communities for fishing, hunting, and gathering natural resources such as cranberry bushes, tamarack, and cedar.

In the 19th century, European settlers transformed the Holland Marsh into one of Ontario's most productive agricultural areas through an engineered drainage system of canals and pumping stations. The Bridge Street Corridor, directly adjacent to and shaped by this infrastructure, emerged as a key access point for the transportation of agricultural goods from local farms to growing urban markets. Today, while the canal system continues to support drainage for farming, it also accommodates recreational uses such

as paddling and informal trail connections along the Holland River and canal network near Bridge Street.

Despite engineered drainage systems, the Corridor remains flood-prone due to its low-lying topography and proximity to the Marsh. In accordance with provincial policy, development in these areas is generally discouraged. Until the Town implements the flood mitigation measures identified in Policy 6.20.4.2 a) vii), development within these flood-prone areas will remain limited.

Nonetheless, the Bridge Street Corridor's unique setting—at the intersection of historic trading routes, farmland, river access, and the regional canal system—offers valuable opportunities to support nature-based and water-oriented recreation. With thoughtful planning, these lands can contribute to a resilient and connected community that respects the ecological legacy of the Marsh while enhancing environmental stewardship and public access.

The policies in this section apply to lands within the floodplain as identified on Schedule B1-A and shall be read in conjunction with the Natural Hazard policies outlined in Section 3.10.5 of this Plan.

- a) The following uses are not permitted within lands located in a flood plain:
 - i) an institutional use including but not limited to places of worship, hospitals, long-term care homes, retirement homes, preschools, school nurseries, day cares, schools and related uses which would pose a significant threat to the safety of the inhabitants if involved in an emergency evacuation situation as a result of flooding or failure of floodproofing measures;
 - ii) an essential emergency service such as that provided by fire, police, and ambulance stations and electrical sub-stations which would be impaired during a flood emergency as a result of flooding or failure of floodproofing measures;
 - iii) uses associated with the disposal, manufacturing, treatment or storage of hazardous substances, including the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of floodproofing measures;
 - iv) New underground parking;

- v) Overnight parking where safe ingress and egress as defined for roadways is not available;
 - vi) Until such time that the measures outlined in policy 6.20.4.2 a) vii) are undertaken by the town, residential uses are prohibited within all areas of the Bridge Street Corridor;
 - vii) The Town shall in consultation with the Ministries of Natural Resources and Municipal Affairs, the County of Simcoe, and the Conservation Authority, conduct the requisite studies and analysis to comprehensively evaluate the natural hazard constraints present on lands within the Bridge Street Corridor and the floodplain and assess the feasibility of establishing parameters for the potential future mixed-use redevelopment of the area.
- b) Development and site alteration shall not be permitted within:
- i) areas that would be rendered inaccessible to people and vehicles during times of flooding hazards, and/or erosion hazards, unless it has been demonstrated that the site has safe access appropriate for the nature of the development and the natural hazard; and
 - ii) a floodway regardless of whether the area of inundation contains high points of land not subject to flooding.
- c) Despite policy 6.20.4.2.1 b), development and site alteration may be permitted in certain areas associated with the flooding hazard where through consultation with the Lake Simcoe Region Conservation Authority and in accordance with the applicable requirements under the Provincial Planning Statement, Lake Simcoe Protection Plan, South Georgian Bay Lake Simcoe Source Protection Plan and the Conservation Authorities Act, if it is determined:
- i) That the development is limited to uses which by their nature must locate within the floodway, including flood and/or erosion control works or minor additions or passive non-structural uses which do not affect flood flows to the satisfaction of the Conservation Authority;
 - ii) The Conservation Authority, along with the Town and relevant agencies, must be satisfied that the proposed development will not create conditions that pose a risk to public health or safety, damage property in the event of a

natural hazard, or impede safe access for people, vehicles, and emergency services during a flood; and,

- iii) The Conservation Authority is of the opinion that the activity is not likely to affect the control of flooding, erosion or unstable soil or bedrock and meets the prescribed requirements and regulations

6.20.4.3 Bridge Street Corridor Lands in Floodplain

Despite the natural hazard policy limitations applicable, Bridge Street Corridor's unique location—at the interface of historic agricultural land, river access, and the regional canal system—presents opportunities to support nature-based and water-oriented recreational activities for the community. With careful planning, these lands can contribute to a resilient, connected community that respects the ecological legacy of the Marsh while enhancing public access and environmental stewardship.

Until the Town undertakes the measures outlined in Policy 6.20.4.2.1 a) vii., steps shall be taken to create a distinct sense of place and identity for the Bridge Street Corridor as the eastern gateway to the Town. Notwithstanding the policies in section 6.20.4.1 on Commercial use area permissions on the Bridge Street Corridor, the following policies apply to all lands within the Bridge Street Corridor:

- a) The Town shall promote enhanced Connectivity and active transportation opportunities through the Bridge Street Corridor by:
 - i) Developing pedestrian and cycling routes along Bridge Street that integrate with the broader active transportation network;
 - ii) Establishing a comprehensive active transportation network that provides direct, safe connections between the GO Station, other public transit stops, corridors, multi-use trails, public spaces, and other key destinations;
 - iii) Promoting safe, accessible pedestrian walkways with sufficient width to accommodate mobility aids.
- b) The Town shall ensure driveways along Bridge Street are minimized to maintain continuous, uninterrupted pedestrian and cycling pathways;
- c) The Town shall encourage the consolidation of vehicle access points where feasible to enhance building frontages,

improve front yard continuity, and reduce disruptions to pedestrian routes;

- d) The Town shall identify and pursue opportunities to enhance or establish new recreational and tourism-focused trail connections within and around the Bridge Street Corridor by:
 - i) Facilitating trail and water-based connections between the Bridge Street Corridor and the historical route of the Carrying Place Trail. This may include the establishment of multi-use pathways through the Holland Marsh that utilize the existing canal dykes where feasible, and public road rights-of-way where canal access is restricted or conflicts with privately owned agricultural lands.
 - ii) Supporting enhanced water-based recreational activities such as canoeing and paddling along the Holland River and its associated canal and dyke systems, with access provided from the Bridge Street Corridor. In alignment with the Town's Trails Master Plan, this may include exploring the development of multi-use trails along reconstructed dykes on both the North and South Canal branches of the Holland River, such as:
 - a. A North Canal route, extending west from Bridge Street and Canal Road, running parallel to Canal Road and the North Canal, and connecting south to the South Canal at West Canal Bank Road (approximately 24 km in total length).
 - b. South Canal route, beginning at Canal Road and West Bank Canal Road, continuing east along South Bank Canal Road and Woodchoppers Lane, then departing the road system to follow the canal before rejoining at Pumphouse Road, ultimately merging with the North Canal trail at Canal Street to create a continuous loop.
 - iii) Recognizing and supporting the cultural and recreational importance of the Marsh Mash Canoe and Kayak Race, one of Ontario's oldest marathon canoe races, which utilizes the canal system surrounding the Holland Marsh.
 - iv) The Town shall collaborate with event organizers, local stakeholders, and Indigenous communities to celebrate the historical significance of the Holland Marsh and its role in regional connectivity and commerce and

community partners to promote the cultural and recreational significance of the canal system and maintain access and infrastructure that supports ongoing public, recreational and traditional uses of the waterways.

- e) Providing or enhancing cycling routes that follow the canal system and connect to the Bridge Street Corridor.
- f) Incorporating support amenities into the trail system, such as trailheads with modest floating docks, rest areas, and interpretive features.
- g) Establishing a primary trailhead entrance along Bridge Street, including self-guided interpretive signage; and
- h) In consultation with the Lake Simcoe Region Conservation Authority, exploring the potential for a “rail-with-trail” cycling route from the Bradford GO Station (Bridge Street and Given Road), extending northward along the railway corridor with the potential to connect to the Scanlon Creek Conservation Area, subject to appropriate design and safety standards.
- i) The Town shall also support and facilitate the establishment and integration of public and privately owned art installations—such as standalone pieces, architectural features, and landscape elements—that enhance the public realm and reinforce the unique identity of the Bridge Street Corridor as the eastern gateway to the Town. Public art should be strategically located and reflect the area’s cultural legacy, including:
 - i) Its role in the historic Carrying Place Trail;
 - ii) Its function as a marketplace for Marsh-grown produce;
 - iii) Its historic significance as a hub of regional connectivity, commerce, and economic activity; and,
 - iv) As part of this effort, the Town shall promote Indigenous placemaking through the incorporation of culturally significant design features and public art co-developed with Indigenous communities. These elements shall celebrate Indigenous cultures, stories, values, and knowledge systems, contributing to a more inclusive and meaningful sense of place.

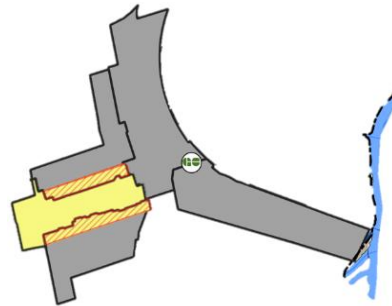
6.20.4.4 Bradford GO Station

The following policies shall apply to the Bradford GO Station located within both the Bridge Street Corridor, with a smaller portion on Dissette Street Corridor.

- a) Parking and passenger pick-up/drop-off areas serving the GO Station shall prioritize universal accessibility and support seamless transitions between automobile use and active transportation;
- b) A comprehensive active transportation network that provides direct, safe connections between the GO Station, transit stops, corridors, multi-use trails, public spaces, community hubs and other key destinations shall be established;
- c) Safe, accessible, and well lit pedestrian walkways with sufficient width to accommodate mobility aids shall be provided to the GO station from key nodes;
- d) Secure and ample bicycle parking shall be provided at GO Station entrances, with sheltered bike storage integrated into the station design and positioned in highly visible locations near platform access points.
- e) Passenger amenities should be integrated into buildings and open spaces adjacent to the transit stop.

6.20.5 Holland Street Corridor

The Holland Street Corridor, consists of approximately 4 hectares of net land area, and forms the central portion of the Town's *Major Transit Station Area* (MTSA), as shown on Schedule B1-A. As one of the first streets established in the former Village of Bradford in the mid-19th century, Holland Street has served as the Town's main street and destination, offering goods, services, and amenities to both local residents and visitors from the surrounding rural communities.



This corridor is home to several historic and heritage buildings that reflect the Town's early settler development and cultural evolution. The conservation, adaptive reuse, and integration of these heritage resources play a vital role in enriching the daily experience of residents and visitors, and contribute to the Town's identity and sense of place. Preserving cultural and natural heritage is a key element of sustainable development

and placemaking. As the Holland Street Corridor continues to evolve, intensification must be carefully balanced with the protection of its heritage properties, viewsheds, and historical identity as a community destination.

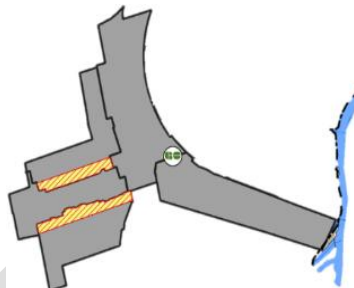
Identified as the second-highest intensification area within the MTSA, the Holland Street Corridor is planned to accommodate growth to the year 2051 with approximately 860 new dwellings, a minimum density of 200 units per net hectare, 1,500 residents, 5,900 square metres of new commercial floor area, and 110 jobs. Future redevelopment shall be guided by a commitment to maintaining the corridor's historical and cultural character, while supporting a vibrant, mixed-use environment that provides diverse housing, jobs, recreational spaces, and community amenities.

- a) Permitted uses within the Holland Street Corridor include:
 - i) High density housing units consisting of mixed use, mid-rise apartments with commercial uses including retail, restaurants, and services, provided at grade;
 - ii) Small-scale commercial, Cultural, art and entertainment uses, community and institutional uses are encouraged at grade in apartment buildings and properties
 - iii) Public uses including public service facilities, infrastructure, parks, recreation/community centres, urban squares, trails with signs to GO Station
- b) Residential developments within the Holland Street Corridor must achieve a minimum density of 200 units per net hectare;
- c) Minimum and maximum heights in Holland Street Corridor shall be in accordance with the height limitations shown on Schedule B-1A of this Plan;
- d) Opportunities for requesting an increase in the maximum permitted height may be pursued subject to the policies outlined in section 4.2.2 d) of this plan;
- e) The Town shall encourage the consolidation of contiguous lots to facilitate comprehensive and coordinated development. Lot assembly is supported where it results in more efficient use of land, improved site design, and enhanced compatibility with surrounding land uses and infrastructure.
- f) Existing buildings on properties that are either listed or designated under the Ontario Heritage Act, or adjacent to such

properties, contained on development sites must be assessed for their heritage value in accordance with policy 3.6.1 of this plan.

6.20.5.1 Holland Street Transition Overlay

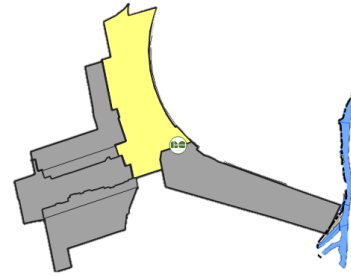
Given the proximity of properties within the Holland Street Corridor Transition Overlay to established neighbourhoods within the residential edges corridors, as shown on Schedule B1-A, the following policies apply to ensure compatibility and seamless integration is maintained between the two corridors:



- i) Residential development within the Transition Overlay shall contribute to achieving the overall minimum density target of 200 units per net hectare for the Holland Street Corridor.
- ii) Development shall incorporate a gradual transition in height, scale, and density from Holland Street toward adjacent low-density residential areas through step-backs, setbacks, and context-sensitive design.
- iii) Higher building heights shall be directed to sites fronting onto Holland Street, with lower-scale, ground-oriented forms permitted within the Transition Overlay to support compatibility with surrounding neighbourhoods.
- iv) A range of built forms and densities shall be provided to ensure a smooth transition between the Corridor and adjacent residential areas.
- v) Site and building design shall mitigate impacts on neighbouring properties by maintaining sunlight access, protecting privacy, and limiting overlook, shadowing, and light spill through appropriate massing, orientation, and screening.
- vi) Developments shall incorporate soft landscaping, fencing, and other buffering treatments to enhance livability and ensure appropriate residential interfaces.

6.20.6 Dissette Street Corridor

The Dissette Street Corridor is located north of the Bradford GO Station and consists of the northern portion of the Major Transit Station Area. The corridor comprises approximately 15 hectares of net land area, including a portion of the passenger pick-up/drop-off area and associated parking lot of the Bradford GO Station, as shown on Schedule B1-A.



Planned as the highest-density and most intensively developed corridor within the Major Transit Station Area, the Dissette Street Corridor shall support a vibrant, pedestrian-oriented and mixed-use community. The Corridor will accommodate the tallest buildings with a diverse mix of residential, office, institutional, and commercial uses. To enhance the public realm and pedestrian experience, developments shall incorporate ground-floor retail, Privately Owned Publicly Accessible Spaces (POPs), parks, trail connections, and wide sidewalks with integrated amenities.

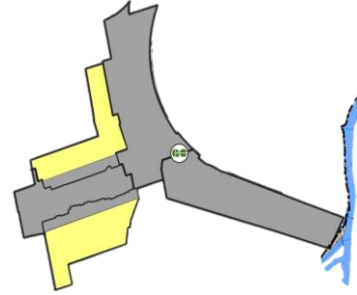
Development in this corridor will have consideration for the presence of existing industrial and major facilities, with the introduction of sensitive land uses to be phased and appropriately mitigated to ensure land use compatibility. Overall, this Corridor is planned to accommodate a total of approximately 3,990 dwellings, a density of 300 units per net hectare, 6,700 square metres of net new commercial floor area, and is expected to support a population of approximately 7,600 people and 121 jobs by 2051. The following policies shall apply to lands within the Dissette Street Corridor:

- a) Permitted Uses within the Dissette Corridor include:
 - i) Legally existing uses and buildings established prior to the adoption of this Plan, however, existing uses are intended to be redeveloped in conformity with this Plan over time;
 - ii) High-density mixed-use developments shall be permitted, consisting of above-grade residential apartments with ground-floor uses fronting Dissette Street that may include retail, restaurants, community services, offices, institutional uses, or public service facilities, such as medical offices or clinics.
 - iii) Residential developments within the Holland Street Corridor must achieve a minimum density of 300 units per net hectare;

- iv) Minimum and maximum heights in the Dissette Street Corridor shall be in accordance with the height limitations shown on Schedule B-1A of this Plan;
 - v) Opportunities for requesting an increase in the maximum permitted height may be pursued subject to the policies outlined in section 4.2.2 d) of this plan;
 - vi) Drive-throughs and new automotive service uses are prohibited;
- b) The tallest buildings shall be concentrated in closer proximity to the Bradford GO Station, where access to community amenities, facilities, transit, and active transportation routes is greatest and transition down toward residential edges;
- c) Buildings in proximity to the Bradford GO Station are encouraged to enhance the public realm through high-quality design elements, including façade articulation, fenestration detailing, landmark architectural features, and the use of durable, high-quality materials. Developments shall also provide clear and direct pedestrian connections to the GO Station to support walkability and transit accessibility.
- d) All development within 300 meters of heavy rail shall comply with the requirements of the Metrolinx/GO Transit Adjacent Development Guidelines;
- e) The following policies apply to the development of lands adjacent to the railway right-of-way:
- i) Development adjacent to the railway corridor may require compatibility assessments to address safety, noise, vibration, and other potential impacts; and,
 - ii) Warning clauses and environmental easements may be required through the planning process to notify future occupants of railway operations and to address operational emissions, particularly for developments within 300 metres of the railway corridor.

6.20.7 Residential Edges (North and South)

The Residential Edges Corridor consists of two separate areas in the northern and southern section of the Major Transit Station Area. This area is characterized by a mixture of established residential neighbourhoods, institutional uses and residential intensification projects.



The southern Residential Edge is also intended to accommodate the future Town Civic Centre. The sites within this area are intended to offer opportunities for a modest increase in density., accomodating a forecasted 1,000 additional residents by 2051. Permitted uses within the Residential Edges Corridor include:

- a) Legally existing uses and buildings established prior to the adoption of this Plan.
- b) A diverse mix of low to medium-density residential housing options including semi-detached dwellings, duplexes, triplexes townhouses, multiple unit dwellings and up to 10-unit walk-up apartments.
- c) Residential developments within the Residential Edges Corridors must achieve a minimum density of 40 units per net hectare.
- d) Parks, urban squares, Privately Owned Public Spaces (POPS), institutional uses including public service facilities, and neighbourhood commercial uses—such as convenience stores, personal service establishments, small-scale eating establishments, and other similar businesses that support the day-to-day needs of the local community—shall be permitted.
- e) Developments shall prioritize strong pedestrian connections, trails and walkways to the Bradford GO Station, adjacent corridors and encourage the integration of parks and open spaces with linkages to the active transportation network.
- f) Institutional uses including public services facilities, Neighbourhood commercial uses such as convenience stores, personal service establishments, small-scale eating establishments, and other similar convenience commercial uses that serve the day-to-day needs of the area.

6.20.8 Major Transit Station Area General Policies

a) Population and Employment

The overall Major Transit Station Area has a gross area of 62 hectares and is planned to with consideration of its existing population it is anticipated to accommodate a total population of 11,500 residents, 15,800 sq. m. of commercial floor area and an estimated total of 575 jobs.

<i>Growth to 2051 Category</i>	<i>2051</i>
<i>Total Population</i>	11,500
<i>Total Jobs</i>	1,300
<i>Total Gross Land Area (Hectare)</i>	62
<i>Residents and Jobs per Hectare</i>	<i>200 people and jobs</i>

b) Transportation

i) Transportation studies/future road networks:

- a. Collector and arterial roads will be designed to balance automobile, pedestrian and cycling activity, to contribute to the vision of a safe, transit-supportive, complete community with a pedestrian-oriented environment.
- b. In order to increase connectivity in the area, the Town will explore the feasibility of new road connections including:
 1. An extension of Scanlon Avenue east to Dissette Street
 2. A new connection between Marshview Boulevard and Canal Road
- c. Privately owned streets and driveways will be designed to be consistent with the overall vision for the transportation network in the MTSA as well as Town engineering standards.
- d. Direct connections from private roadways to collector and arterial roads will only be permitted for shared access to a condominium block or to parking access for larger sites. Multiple individual connections from private roads or lanes shall be predominantly accessed from local roads.

ii) Active transportation network

- a. Development in the Bradford GO MTSA will maximize the connectivity of the street network for pedestrians and cyclists. This will be achieved by:
 - 1. Ensuring that gaps in the street network are minimized by the provision of strategically located sidewalk and pathway connections
 - 2. Seeking opportunities to enhance grid connectivity between new developments and the surrounding pedestrian and bicycle infrastructure
 - 3. Providing high-quality bicycle parking in safe, conveniently located areas.

iii) Transit facilities

- a. The Town will leverage existing transit connections to improve transit access between the GO Station and other parts of Bradford West Gwillimbury.

iv) Parking

- a. Notwithstanding any other policies in this Plan, development applications on lands located with the Bradford GO Major Transit Station Area will not be subject to minimum parking requirements.
- b. The Town encourages the redevelopment of surface parking lots along Holland and Dissette streets in order to achieve the minimum density target.

v) Housing

The Province of Ontario has authorized the use of Inclusionary Zoning within Major Transit Station Areas to require the provision of affordable housing units within new developments. To support the implementation of Inclusionary Zoning within the Bradford Major Transit Station Area:

- a. The Town shall conduct an Assessment Report, in accordance with provincial requirements, that analyzes demographics, income levels, housing supply, housing need and demand, average market prices and rents, and the potential impacts on the housing market. The findings of this report shall inform the consideration and potential implementation of Inclusionary Zoning within the Major

Transit Station Area through future secondary planning, equivalent processes, and subsequent zoning by-law amendments.

vi) Public Realm

- a. Development shall provide direct, safe, and legible pedestrian and cycling connections to the Bradford GO Station and adjacent transit facilities, incorporating wayfinding features, weather protection, and defined gateways to enhance the user experience.
- b. All development shall be compact in form, pedestrian-oriented, and designed to frame the public realm with active frontages, frequent pedestrian entrances, and strong connections to surrounding open spaces and transit infrastructure.
- c. Streets, boulevards, and rights-of-way shall be designed to support multi-modal mobility and high-quality public spaces, including continuous sidewalks or multi-use paths on both sides of the street, connected cycling infrastructure, pedestrian-scaled lighting using full cut-off fixtures, and street trees or landscaping for shade, comfort, and visual interest.
- d. Development shall incorporate public or semi-public gathering spaces—such as plazas, urban squares, or Privately Owned Public Spaces (POPS)—that promote community interaction, provide social and recreational functions, and activate the corridor year-round.
- e. A fine-grained, permeable street network with shorter blocks and frequent, controlled pedestrian crossings shall be encouraged to promote walkability and support safe, flexible travel options.
- f. Roadway design shall integrate traffic calming measures and prioritize pedestrian and cyclist safety while maintaining visually attractive and functional streetscapes.
- g. The public realm shall connect to an integrated system of parks, trails, and open spaces, providing year-round opportunities for walking, cycling, recreation, and ecological connections to the Holland Marsh, canal system, and surrounding neighbourhoods.

- h. Development shall contribute to cohesive, high-quality streetscapes through the use of durable materials, coordinated street furnishings, and inclusive design features that support universal accessibility and equitable use of public

6.20.9 Implementation and Monitoring

- a) The boundaries of the Bradford GO Major Transit Station Area as shown on Schedules B1 and B1-B, are subject to minor refinements which will not require an amendment to this Plan, provided that the achievement of the overall Vision for the area is not negatively affected.
- b) The minimum density requirement is measured within all of the lands delineated in the MTSA, inclusive of any environmental features, constraints, and rights-of-way for transportation or other linear infrastructure.
- c) The minimum density requirement will be achieved based on all existing uses and new development over the long-term. Individual developments within the MTSA are not required to meet the minimum target.
- d) All development applications shall demonstrate how proposals support the overall gross density target for the MTSA as well as the Town's long-term Vision for the area.