



# Bradford Highlands

## URBAN DESIGN REPORT

### TOWN OF BRADFORD WEST GWILLIMBURY

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# EXECUTIVE SUMMARY

The Bradford Highlands Urban Design Report provides a development vision, a set of design principles, detailed design guidelines, and an implementation strategy to direct the creation of a high quality, cohesive and complete residential community. The document aims to provide comprehensive design guidance on the development that supports initiatives set out in the Official Plan and the Town-Wide Urban Design Guidelines August 2022, focusing on the physical design of the community, with particular reference to structuring elements, the road network, Natural Heritage System (NHS), parks, stormwater facilities, residential areas, and institutional area.

This document prescribes open space and built form guidelines and principles while allowing flexibility for delivering variety of architectural form and design expression that will provide interest in the urban environment. The information contained herein will guide the community design to ensure that new development is compatible with the character of the existing local community, respects and enhances the natural heritage features of the area, creates an attractive and safe pedestrian realm, supports active transportation and recreational activities, and is environmentally sustainable.

# SECTION 1

## INTRODUCTION

### 1.1 Purpose and Intent

This Urban Design Report (UDR) has been prepared on behalf of Bradford Highlands Joint Venture for their proposed Draft Plan of Subdivision, also known as Bradford Highlands, located on the former Bradford Highlands Golf Club lands in the Town of Bradford West Gwillimbury. The intent of this UDR is to establish site specific architectural and urban design criteria which support design principles established in the Bradford West Gwillimbury Official Plan and the Town of Bradford West Gwillimbury Town-Wide Urban Design Guidelines while allowing flexibility to deliver a range of design expressions and innovation. The UDR also forms the basis for an architectural control review process as required by the Town of Bradford West Gwillimbury.

The information contained herein describes the physical design of the community and is intended to promote new development that:

- is appropriate to its local context within the community of Bradford;
- reinforces a vibrant, walkable and transit supportive community;
- achieves design excellence by enhancing built form and the public realm;
- respects and enhances the natural heritage features of the area;
- supports active transportation and recreational activities; and,
- is environmentally sustainable.

Images and diagrams contained in this document are conceptual in nature and provided for illustrative purposes to demonstrate the intended guideline or design principle. They should not be construed literally as the final product or as the only manner in which the intended guideline or design principle can be implemented. Refinements to the concepts contained herein may occur based upon the Town's review of the detailed engineering and landscape submissions.



Promoting architectural variety, innovation and quality with complementary landscape and open space features





Promoting safe, pedestrian-friendly and attractive streetscapes

## 1.2 Objectives

The objectives of the Urban Design Report are:

- Demonstrate how the proposed subdivision appropriately integrates with the existing/ planned built form, streetscape, road network and structure of the surrounding community's urban fabric.
- Respect and enhance cultural / natural heritage features within and abutting the subject lands.
- Provide design guidelines that will establish a positive visual character and promote a consistently high standard for built form and public realm elements.
- Promote vibrant, pedestrian-oriented, transit-supportive streetscapes that promote a sense of place and contribute to a complete community.
- Provide direction for building forms that will help to sustain local and regional transit initiatives.
- Promote variety among building designs within an established vocabulary of architectural styles, forms, materials, and details.
- Encourage a safe community by adopting principles of Crime Prevention Through Environmental Design ("CPTED").
- Establish design requirements for buildings in prominent locations (Priority Lots).
- Minimize the visual impact of garages and parking areas within the streetscape.
- Establish requirements for the appropriate siting of buildings according to type, size, style and location within the subdivision.
- Establish procedures for an architectural control process.



# SECTION 2

## COMMUNITY CONTEXT

### 2.1 Location

The Bradford Highlands subdivision comprises an area of 60.0 hectares (148.26 acres) within the southwest portion of Bradford. The subject lands were a formerly used as a golf club (Bradford Highlands Golf Club), which is situated on the south side of Line 6, east of Brownlee Drive.



Figure 2.1a: Bradford Highlands Joint Venture subdivision location and surrounding context



## 2.2 Site Context and Existing Character

The site is bounded by:

- West - Existing estate residential fronting onto Brownlee Drive, partial frontage along Brownlee Drive, and agricultural lands.
- East - Existing residential fronting Inverness Way and Golfview Boulevard, and Environmental Protection lands containing the Holland Marsh West Canal.
- North - Existing rural residences fronting onto Line 6 and partial frontage along Line 6; opposite is an existing residential subdivision.
- South - Existing rural residences fronting onto 5th Line, partial frontage along 5th Line and the Holland Marsh West Canal.

Site topography contains gentle to moderate grades with the highest elevation occurring in the northwest corner of the site and gradually descending towards the south along the Holland Marsh West Canal. The subject lands were formerly used as a golf course with multiple fairways defined through scattered clusters of vegetation and small-scale ponds. The southern portion of the site contains the Holland Marsh West Canal which is lined with heavy vegetation. This area will be preserved and integrated into the community as an Environmental Protection block.

Currently, there is an existing transit route (BWG Transit Route 2A) along Line 6 abutting the subject lands. This transit route contains two bus stops along Line 6 that are in close proximity to the site. In addition, the Bradford GO Station is located approximately three kilometres northeast of the site. Other future transit options are currently being explored to service the community.

The Bradford Highlands subdivision will become an important component of a complete community, given that it is situated in close proximity to a range of amenities such as public transit, bike routes, trails, parkland, employment lands, commercial / service facilities, recreational and community uses. These features will contribute to creating a walkable and transit-supportive community that provides residents with the opportunity to be less dependant upon automobile usage.



Views of the site from Line 6



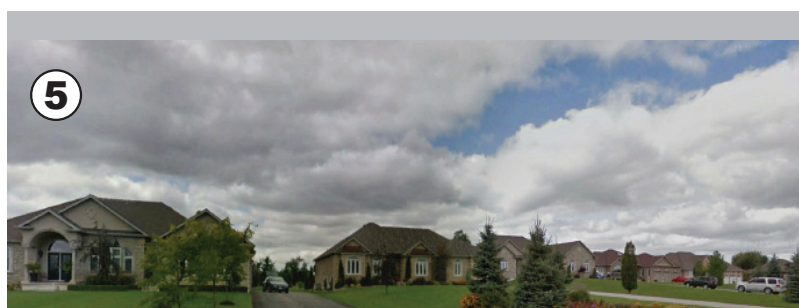
Views of existing residential subdivision to the east from Inverness Way



View of the clubhouse and parking area associated with the former Bradford Highlands Golf Club from Brownlee Drive.



Views of existing residential on Brownlee Drive



Views of existing residential on Golfview Boulevard



### 3.1 Policy Context

The proposed Bradford Highlands Joint Venture development is subject to a number of planning documents and processes at the provincial, regional and local government levels that will guide its design. This UDR outlines a set of guidelines consistent with the objectives of the following documents:

#### Provincial Policy Statement

- The Provincial Policy Statement outlines the province's goals and objectives related to community planning and growth and aims to achieve development in a healthy, economically, and environmentally sustainable manner.
- Planning for the proposed Bradford Highlands subdivision takes into consideration with the policies of the Provincial Policy Statement. However, currently the subject lands are located outside the Bradford Urban Area and requires a revision to the settlement boundary. Details regarding the planning processes and justification for including the lands into the Settlement Area are provided in the Planning Justification Report.

#### Growth Plan for the Greater Golden Horseshoe

- The Growth Plan is a provincial document which supplements the Provincial Policy Statement, provides an overall vision and direction for residential and employment related development within one of the fastest growing regions in North America.
- The Growth Plan establishes a long-term vision for growth in the area, and advocates for the development of vibrant, compact and complete communities that support a strong economy through intensification of the existing built-up areas.
- The proposed Bradford Highlands subdivision, once added to the Settlement Boundary, will be located within a designated growth area and, in this regard, is in keeping with the intent of the Growth Plan.

#### Simcoe County Official Plan

- The Simcoe County Official Plan implements the directions of the Provincial Policy Statement and the Growth Plan through local planning policies and outlines a long term vision for Simcoe's physical form and community character.
- The subject lands are designated "Rural" and "Greenbelt Plan - Protected Countryside" on Schedule 5.1 - Land Use Designations.
- In 2017, the applicant submitted an Official Plan Amendment for the subject lands to be brought into the Bradford Settlement Area, which is under Town review. The applicant plans to make a re submission of the Official Plan Amendment concurrent with the Draft Plan of Subdivision Application and Zoning By-law Amendment. Details regarding this process and rationale is provided in the Planning Justification Report.
- Notwithstanding the above, the proposed development is consistent with the County's planning vision.

### Town of Bradford West Gwillimbury Official Plan (2002 OP and 2021 OP)

- The Town of Bradford West Gwillimbury Official Plan provides a vision for the development that promote new developments to achieve exemplary standards for built form, streetscapes and interface with open space features. The design of the Bradford Highlands subdivision promotes exemplary urban design standards through:
  - a) The creation of a strong community framework and identity;
  - b) The creation of distinct yet integrated neighbourhoods;
  - c) The creation of a transitional interface between urban and significant natural features;
  - d) A compatible and complementary interface between existing and new development; and,
  - e) Pedestrian-oriented neighbourhoods and streetscapes.
- Presently, the subject lands are located outside the Bradford Urban Area, and are designated "Rural" as per Schedule B - Land Use.
- As previously discussed, the 2017 Official Plan Amendment for the subject lands will be resubmitted together with the Draft Plan of Subdivision Application and Zoning By-law Amendment for Town review. The Official Plan Amendment seeks to redesignate the "Rural" land use to "Residential" use that may include "Low Density Residential" and "Medium Density Residential", as well as "Greenbelt Natural Heritage System".
- The proposed development of the subject lands and this UDR will respect the pertinent development and urban design policies, specifically Section 3.10 - Town-Wide Design of the Bradford West Gwillimbury Official Plan (2021 OP).

### Town of Bradford West Gwillimbury Zoning By-Law 2010-050

- The subject lands are currently zoned Open Space Recreational "OSR", Agricultural "A", Marsh Agricultural "AM", and Natural Heritage System Two "NHS2".
- The applicant will be submitted an application to amend the zoning by-law to permit the proposed residential, institutional, and open space uses.
- Details regarding the ZBLA can be found in the Planning Justification Report.

### Town of Bradford West Gwillimbury Town-Wide Urban Design Guidelines

- The Town-Wide Urban Design Guidelines provide a starting point for a discussion about urban design, site planning, built form, and open space concepts and principles for various development situations, including new neighbourhoods.
- Section 3.1 - New Neighbourhoods establishes a high-level framework of design criteria for the overall identity and structure of the proposed community, as well as for the appearance of new buildings/ built forms, streetscape, parkland and open spaces.
- The public and private realm elements within the Bradford Highlands subdivision will be designed in conformity to the applicable objectives and criteria outlined in the Town-Wide Urban Design Guidelines and the policies of the Official Plan.
- The Town-Wide Urban Design Guidelines, in conjunction with this UDR will be used by the Town in their review and evaluation of the various development proposals within the subject lands.

# SECTION 4

## DESIGN VISION

### 4.1 Design Vision

The Town-Wide Urban Design Guidelines provides the following design vision for the community:

“to achieve a high standard of physical design and architectural quality in pursuit of a sustainable, innovative community. New development is encouraged to be similar in character to adjacent existing development while achieving the overarching objectives of the Official Plan.”

The design vision for the Bradford Highlands Joint Venture subdivision builds from the overall vision stated above by providing:

- a. a cohesive mix of housing types and densities to support higher order transit and provide choice of accommodation options;
- b. a robust multi-modal and active transportation network;
- c. a fine-grained road pattern; and,
- d. a range of open space amenities supporting healthy lifestyles and enhancement of the natural features of the local environment.

These features will support the emerging urban character of the area and contribute to a complete community where residents can live, work and play. The use of distinctive streetscape / public realm elements, high quality architecture, tree lined streets and landscaped community features will help to reinforce the design vision for the Bradford Highlands subdivision by providing an attractive urban identity and creating a distinct sense of place within the Town of Bradford West Gwillimbury.



Conceptual images of the development vision for Bradford Highlands



## 4.2 Guiding Principles

The proposed subdivision has been thoughtfully planned to implement the vision for the community. The design and structure of the Bradford Highlands subdivision reflects the following guiding principles established within the Town-Wide Urban Design Guidelines:

### 1. *A distinctive community character and a cohesive, consistent quality of architectural style*

Response:

- Provide variety in built form, employing a diversity of materials and styles that reinforce a high quality character.
- Create varied setbacks and ensure adjoining uses are compatible with one another.
- Establishing parameters for an Architectural Control process to ensure a cohesive and high-quality built form environment.

### 2. *High quality of architecture and site design for commercial and institutional uses*

Response:

- No commercial or institutional uses are proposed within the community.

### 3. *Coordinated and attractive streetscapes*

Response:

- Ensure that built form and streetscape elements contribute to creating a visually attractive and comfortable pedestrian environment.
- Control the visual impact of garages and parking areas along the streetscape.
- To provide design criteria for dwellings in prominent locations (Priority Lots such as corner lot dwellings, view terminus dwellings, dwellings that require rear and/ or side architectural enhancements).



A distinctive community character and a cohesive, consistent quality of architectural style



Coordinated and attractive streetscapes



#### 4. *Visually prominent and easily accessible parks and open spaces features*

Response:

- Locating parks and stormwater management facilities in visually prominent areas within the community with direct street frontage.
- Siting parks adjacent to other open space or institutional uses.
- Integrating trails within the stormwater management facilities that may provide access to adjacent NHS trails or parks.
- Providing a linked pedestrian and cycling system together with a permeable modified-grid road pattern will promote healthy active transportation opportunities throughout the community.

#### 5. *A network of parks, streets and open spaces as the predominant features and amenity of the Town*

Response:

- Create a hierarchy of open spaces.
- Provide both active and passive recreational spaces.
- Provide sufficient park space within easy walking distance of residential uses.
- Incorporate natural features as key elements of the neighbourhood.
- Define neighbourhood edges through a variety of plantings and landscape treatments.
- Design edge features to mitigate traffic noise while contributing to the character and identity of the neighbourhood.
- Design stormwater management facilities as neighbourhood amenities, where possible.
- To provide attractive streets with an emphasis on well designed and pedestrian-oriented streetscapes and a significant tree canopy.



Visually prominent and easily accessible parks and open spaces features



A network of parks, streets and open spaces as the predominant features and amenity of the Town

# PROPOSED DEVELOPMENT

## 5.1 Proposed Development

The Bradford Highlands subdivision is a greenfield development whose design aligns with the surrounding residential neighbourhoods to become an integral component of the Bradford Community. It will comprise low and medium density residential uses, environmental protection lands, park spaces, and stormwater management ponds. A series of interconnected parks, open space elements, environmental protection areas, and multi-use trails will serve as active transportation and recreational amenities.

The main structuring elements of the Bradford Highlands Subdivision Plan are described below:

### Natural Heritage System

- Heavily treed environmental lands will form part of the NHS in the southeast portion of the subdivision adjacent to the Holland Marsh West Canal.
- A 0.42 ha. compensation block is provided in the southern portion of the subdivision adjacent to Park Block 509.

### Other Open Space Features

- Two parks are located in the southern and central portions of the community. These parks will be 1.46 ha. and 1.60 ha. in size. Their programming and design will be determined in consultation with the Town staff to provide a balance of facilities and passive and active recreation opportunities throughout the community and to ensure participation and use for all ages and abilities.
- Two stormwater management ponds located in the southeast and central portions of the subdivision.
- A series of drainage blocks generally located in the southern portion of the development.
- A comprehensive, integrated trail and open space network to support a walkable, pedestrian- and cycle-friendly community.

- This interconnected system of open spaces and amenities is strategically planned and placed within the community to offer ease of walk-to access for residents and to expand upon, buffer and protect the features of the NHS.

### Road Hierarchy

- A modified grid road network with varied block lengths provides connectivity within the neighbourhood and responds to the existing adjacent arterial and local roads.
- Primary accesses to the site will occur from Line 6 and 5th Line via Street 'A', the proposed collector road. A series of local streets that branch from the collector road system is proposed.
- Streets 'B' and 'C' also provide access to the site from the adjacent residential development northeast of the site. Street 'R' will provide a future access to lands west of the site.
- A portion of Brownlee Drive will be utilized as frontage for proposed residential lots.

### Low-Rise Residential

- A variety of low-rise residential dwelling types are strategically planned throughout the community including:
  - Single detached dwellings;
  - Street townhouse dwellings; and,
  - Back-to-back townhouse dwellings.
- The range of housing options provided will cater to various ages, incomes and household sizes.



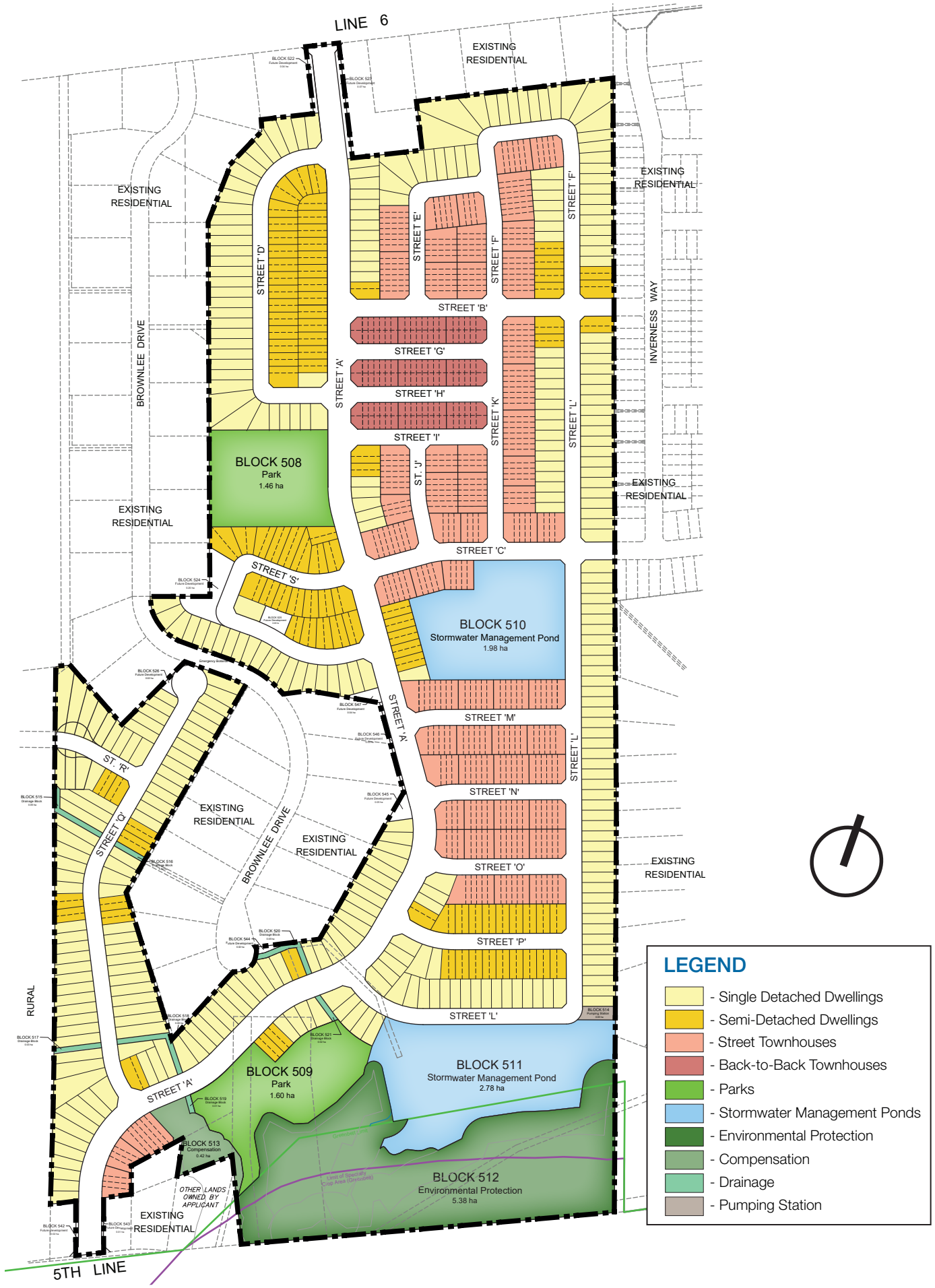


Figure 5.1: Bradford Highlands Joint Venture Development Plan

## PUBLIC REALM GUIDELINES

The public realm is a vital component of the Bradford Highlands subdivision that will work together with private realm built form to assist in the efficient functioning of the community and defining its character. The design of the public realm should reflect a high standard of quality and relate to the surrounding context, land uses, and landmarks so that networks of public open spaces are created that facilitate social and civic interactions.

A successful public realm provides:

- A functional, safe, sustainable, and enriching environment.
- Well articulated streetscapes with landscaped boulevards.
- A network of streets that accommodate multi-modal choices for pedestrians, cyclists, transit and vehicles.
- Pedestrian linkages that connect the residential population to the, open spaces, and adjacent neighbourhoods.
- Well-designed street furnishings and way finding that provide orientation, identity and a sense of place.

## 6.1 Community Safety

Crime Prevention Through Environmental Design (CPTED) is an approach to crime prevention that takes into account the relationship between the physical environment and the users of that environment. The application of CPTED to urban design assists in the creation of spaces that are perceived as being safe. Public spaces and buildings should be designed to encourage interaction between neighbours and a sense of community in order to deter criminal activity. The following should be considered in the design of this new development to foster a safe community:

- A clear definition between public and private space should be provided through the design and placement of buildings, fencing and landscaping.
- Walkways and landscaping should direct visitors

to the building entrance and away from private areas.

- Buildings should be designed to enhance observation of public areas, (streets, parks, walkways, etc.) including ample fenestration to promote natural surveillance or “eyes on the street”.
- Adequate lighting should be provided along streets, lanes, sidewalks, public walkways and parking areas to ensure pedestrian comfort and safety. Site lighting should be directed downward and inward to mitigate negative impact on neighbouring uses.
- Front porches should be provided to promote natural surveillance and serve as an interface between private and public realms.
- Main entrances to the building should be well lit and clearly visible from the street or other publicly accessible areas. Avoid building entries which are deeply recessed or hidden from the street.
- The street address should be clearly visible from the street with numbers a minimum of five inches high that are made of nonreflective materials.

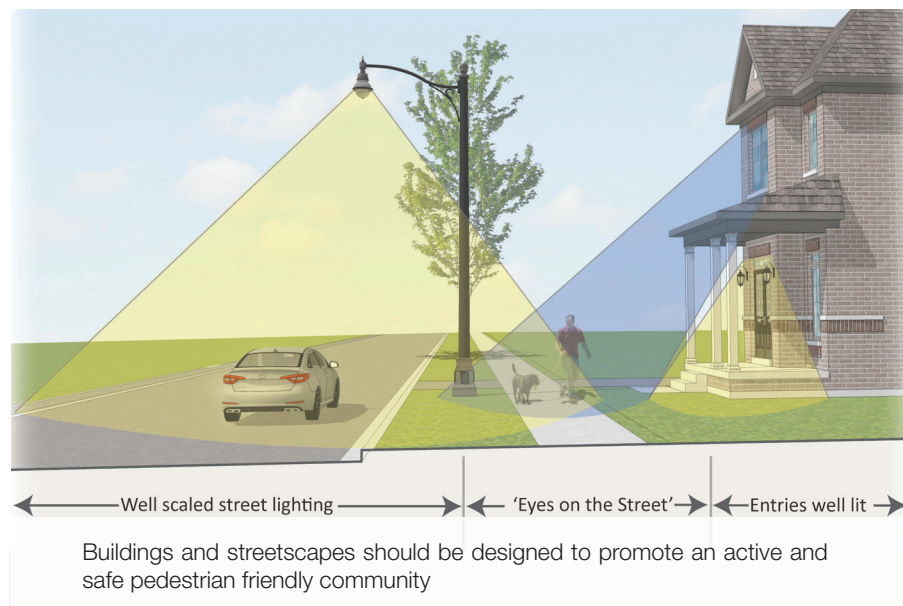






Figure 6.0: Bradford Highlands Joint Venture Landscape Master Plan - Prepared by Landscape Planning Landscape Architects

The Bradford Highlands subdivision provides a modified grid hierarchy of new streets that responds to the site's topography, natural features, and future / existing uses along the community's edges.

## 6.2.1 Provisions For All Streets

Site circulation will be facilitated through a coherent network of public roads, sidewalks and trails for the safe and convenient movement of pedestrians, vehicles and cyclists and to reinforce the vision of a pedestrian-oriented neighbourhood with multiple linkages, transit and active transportation opportunities. All streets consist of two components - the public realm which is the area extending within the road allowance, and private property which extends beyond the road allowance.

- Street elements such as light standards, street furnishings and signage should be combined and coordinated where appropriate, to create consistency and continuity both in design and placement.
- Ensure pedestrian-scaled and energy efficient lighting for all streets.
- In order to create a continuous and uniform canopy on both sides of the street, street trees and sodded (or hard surface) boulevards shall be provided in accordance with Town standards.
- A variety of fencing options may be provided. Details for fencing design and location will be provided in the Landscape Plan for the subject lands.
- Street name signage shall be incorporated to facilitate orientation and wayfinding.
- On-street parking will occur on public streets, wherever feasible, to reduce vehicle speeds, animate the street and serve as a buffer between pedestrians and moving vehicles.
- All street elements shall be designed in accordance with Town of Bradford West Gwillimbury standards.
- Refer to R.O.W. cross sections on the following page for details.

## 6.2.2 Arterial Roads

- Line 6 is classified as an Arterial-Major Road that frame the north edge of the subdivision.
- Arterial roads should promote active transportation, pedestrian-oriented development, public transit opportunities and transit-friendly facilities while maintaining the vehicular mobility function.
- Existing transit stops are located along Line 6 associated with BWG Transit Route 2A.
- Currently, sidewalks are provided on both sides of Line 6. However, as indicated in the Town of Bradford West Gwillimbury Transportation Master Plan (July 2022 Final Report) a multi-use path is proposed along Line 6.
- Access to Line 6 from the Bradford Highlands subdivision will occur from Street 'A'.

## 6.2.3 Collector Road

- Collector roads provide important connections between residential neighbourhoods and community functions, such as parks, and other facilities. They largely define the community structure and serve as the primary inter-neighbourhood circulation routes.
- 5th Line is an existing collector road that frames the southern portion of the subdivision. Currently, this road is designed with a rural profile (ditches and culverts). Any upgrades to the road, if proposed will in accordance with Town standards.
- Street 'A' will have a 26.0m R.O.W. that includes 14.0m of pavement (14.6m curb to curb), 2 driving lanes, on-street parking on both sides, and sidewalks on both sides.
- Street 'A' is the only collector road proposed within the subdivision and will run north-south creating a linkage between Line 6 and 5th Line.



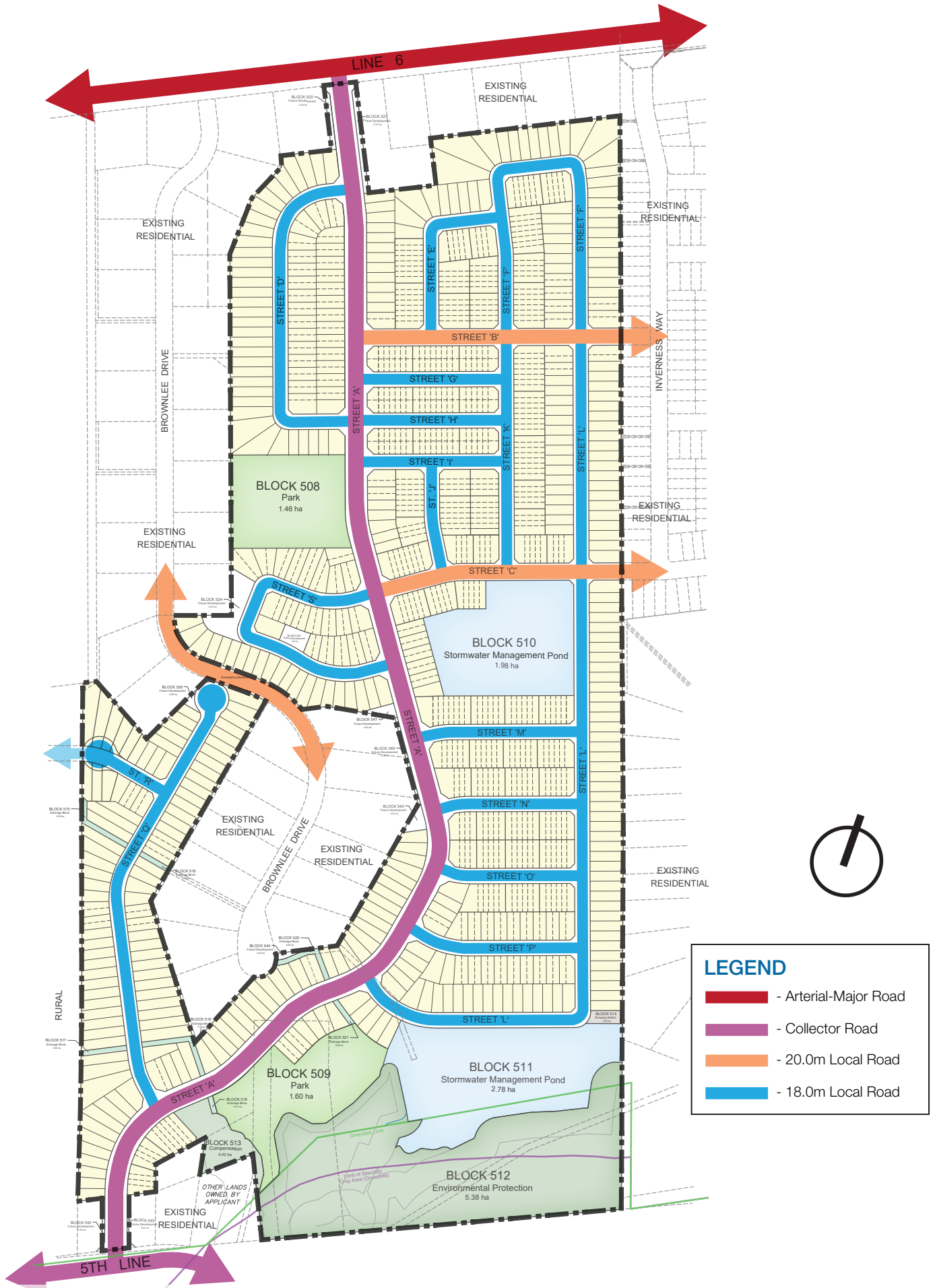


Figure 6.2: Bradford Highlands Joint Venture Street Hierarchy Plan

## 6.2.4 Local Roads

- Local roads have been designed to form a modified grid street network.
- Local roads serve residential neighbourhoods and are intended to provide a comfortable pedestrian experience with relatively low levels of local vehicular traffic.
- Local roads will have right-of-way widths of 18.0m and 20.0m. The 18.0m local road R.O.W. will accommodate a sidewalk on one side of the street. the 20.0m local road R.O.W. will have sidewalks on both sides of the street and will provide access to the existing residential subdivision northeast of the site.
- The sidewalk should generally be located on the north and east side of the street to receive sunlight to facilitate the melting of snow and ice or where deemed desirable from a pedestrian connectivity standpoint.
- Lots 371 - 376 will front onto Brownlee Drive. Brownlee Drive is an existing local road generally located west of the site. This road currently has, and will maintain its rural cross section (ditches and culverts). Where new driveways are proposed, culverts will be provided to permit driveway access to the proposed lots.
- Garages and driveways may face and access local roads.
- All local roads will accommodate on-street parking.

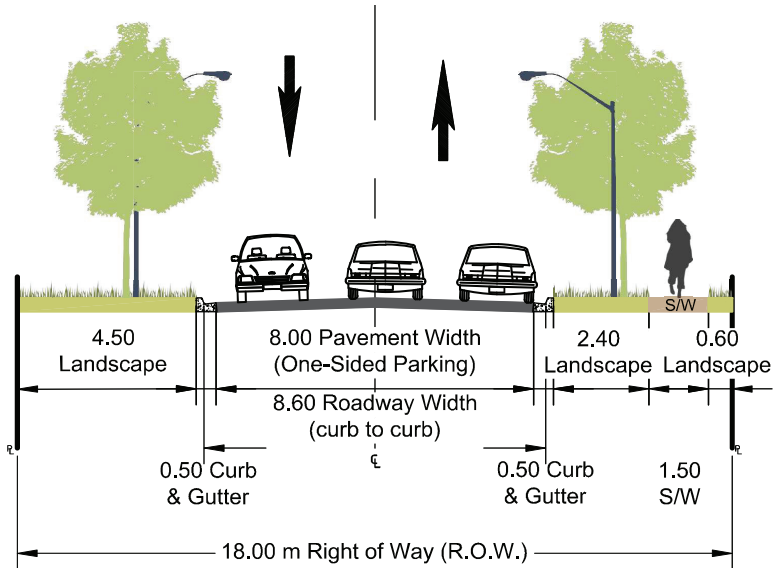


Figure 6.2.4a: Conceptual cross section for 18.0m Local Road

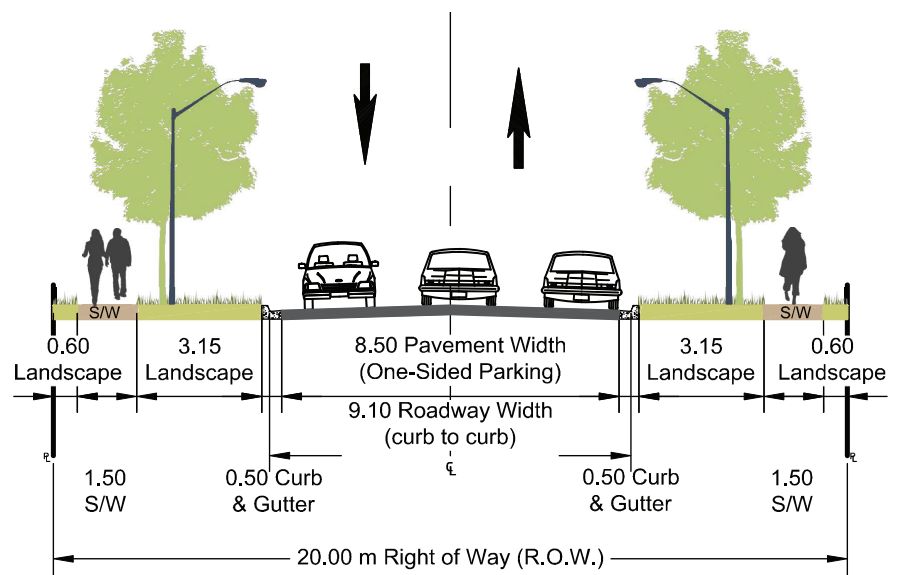


Figure 6.2.4b: Conceptual cross section for 20.0m Local Road

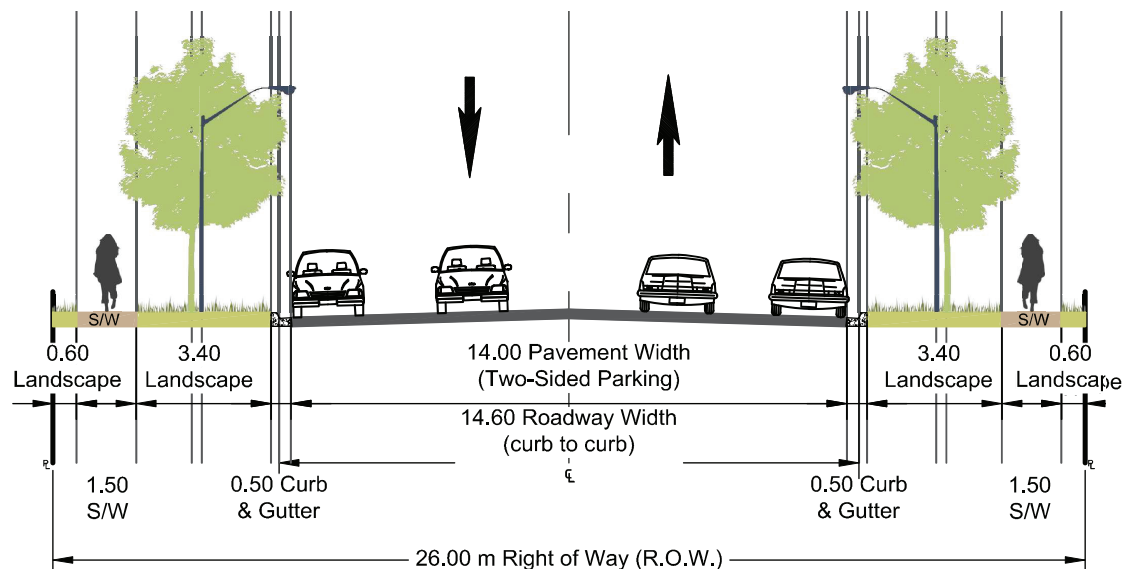


Figure 6.2.4c: Conceptual cross section for 26.0m Collector Road (Street 'A')



## 6.3 Active Transportation

A major factor in creating a healthy, walkable and complete neighbourhood will be offering mobility choices that promote pedestrian and cyclist connectivity, comfort and safety. Provision of public sidewalks, multi-use paths, and off-street trails will offer pedestrians and cyclists alternatives to vehicular travel through the community. The active transportation network within the proposed subdivision will provide safe, attractive, and convenient access to community focal points and open spaces on a local and community scale, for both commuter and recreation purposes.

- As identified in the Town of Bradford West Gwillimbury Transportation Master Plan (July 2022 Final Report), a multi-use path is proposed along Line 6 along the northern edge of the subject lands.
- The multi-use path shall be designed in accordance with all applicable accessibility standards and will be constructed by others.
- Proposed trails will link public open spaces through the street and sidewalk network to form a continuous, complete and pedestrian-friendly public realm.
- Additional pedestrian connectivity will be established with the proposed sidewalk system.
- All sidewalks are to be designed and located as per municipal requirements.
- Public open spaces shall be linked through the street and sidewalk network to form a continuous, complete and pedestrian-friendly public realm.
- Streetscape elements, pedestrian-oriented spaces, landscaping and interesting architecture will be used to create a safe and comfortable environment that promotes active transportation.
- The following design standards should be applied for active transportation routes:
  - SWM Trails: 3.0m wide with asphalt finish. These trails will also serve as SWM maintenance access routes where applicable.
  - Sidewalks: 1.5m wide.
  - Pathways within the parks should be a minimum 3.0m for maintenance purposes.
- Refer to the Bradford Highlands Joint Venture Active Transportation Plan on the following page for details.



Community design and linkages to the proposed multi-use trail network provides opportunity for active transportation



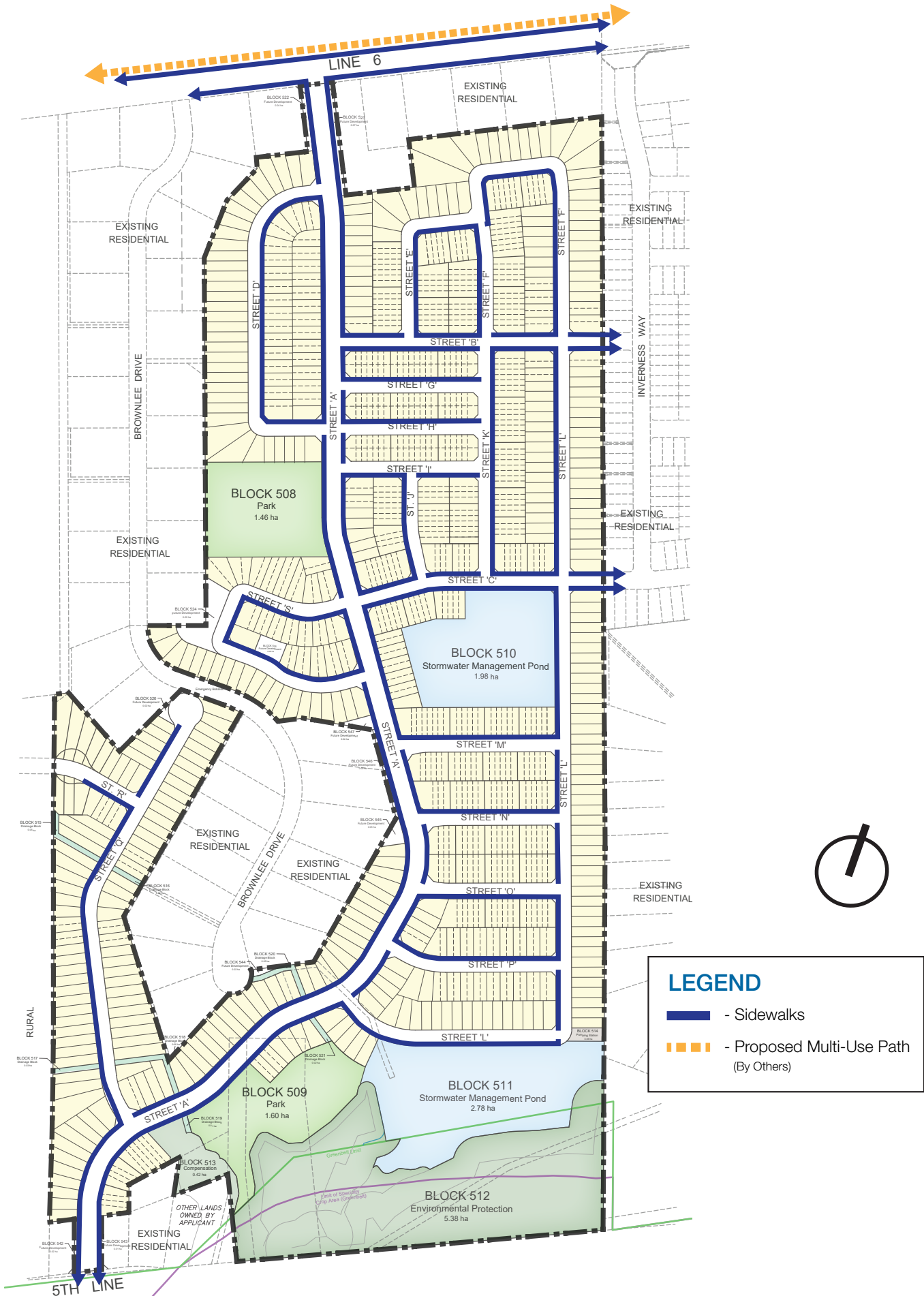


Figure 6.3: Bradford Highlands Joint Venture Active Transportation Plan

## 6.4 Open Space Elements

### 6.4.1 Environmental Protection Block

- The Environmental Protection block is located in the southeast portion of the subdivision, contributing to the extensive Natural Heritage System (NHS) within the community. This feature contains a heavily treed area along the banks of the Holland Marsh West Canal.
- The preservation of this area is to aid in protecting wildlife habitat and connect natural features, while improving community diversity and water management. Through this framework, the Environmental Protection Lands remain protected while providing healthy and sustainable open space features within an urbanized setting.
- Existing vegetation within the Environmental Protection block shall be preserved. Restoration plantings and monitoring conducted as part of an Edge Management Plan may be required in consultation with the Town, County, and Conservation Authorities.
- The Environmental Protection block will be buffered from residential development through the proposed park, stormwater management facility, and compensation blocks.
- A trail system may occur within the park and stormwater management facility adjacent to the Environmental Protection block.
- Where residential lot/ blocks interface with the compensation block, a black chainlink fence should be provided to delineate the public and private spaces. Gates to open space features from individual residential lots are prohibited. Architectural upgrades to rear and/ or side elevations of dwellings backing or flanking onto highly visible open space areas will be required, unless obscured by dense vegetation.



Conceptual images of the environmental protection lands



## 6.4.2 Stormwater Management Facilities

- Two SWM ponds are provided - one in the southeast portion of the subdivision, the other in the east-central portion of the subdivision.
- A naturalized, low maintenance approach to design (layout and planting) should be adopted in the development of SWM ponds and the channel. Aquatic, riparian and upland planting zones which use native trees and shrubs and seed mixes should be specified. Proposed plantings shall comply with Town of Bradford West Gwillimbury and the local conservation authority standards.
- Look-out features/seating areas, incorporating decorative paving and site furnishings, to be provided within SWM blocks in appropriate locations in accordance with Town standards. These features should reflect the aesthetics and character of landscape elements and built form provided within the community.
- Trails provided within the SWM blocks should provide recreational opportunities and linkages to the community's open space network.
- The interface between rear and/or side yards of adjacent residential lots/ blocks and the SWM ponds shall consist of black vinyl chainlink fence. Architectural upgrades to rear and/or side elevations of units backing or flanking publicly visible areas will be required.



Conceptual image of the stormwater management pond



Example image of housing flanking a stormwater management pond



### 6.4.3 Parks

Two parks are proposed within the Bradford Highlands subdivision - one in the southwest portion of the subdivision, the other in the west-central portion of the subdivision. Parks provide central green spaces that will serve as a key recreational and gathering space for neighbourhood residents.

#### Parks

- A 1.46ha (3.61ac) Park (Block 508) is situated on the west side of Street 'A' between Streets 'D' and 'S'. The park will have direct frontage onto Street 'A'.
- A 1.60ha (3.95ha) Park (Block 509) is situated on the south side of Street 'A' between Streets 'L' and 'Q'. This park will have frontage along Street 'A' and is adjacent to open space features to the south, east, and west (Compensation, SWMP, and Environment Protection blocks).
- The parks will provide recreational open space use and serve as focal points for the surrounding neighbourhood. They will be characterized by a mix of open green spaces for passive and active play, seating amenities with shade structures, and a variety of recreational features.

The following guidelines should be considered:

- Predominantly soft landscaping should be utilized allowing for a variety of active and passive use opportunities that serve the surrounding neighbourhoods.
- Provide reasonably level and functional open play areas for passive recreation use.
- All aspects of the park design should reflect the goals of integrated, inclusive programming and universal accessibility through barrier-free design, wherever practicable.
- Shade structures and playgrounds should be unique in character and designed as major focal elements.
- Entry points to the parks shall be strategically located to ensure convenient access and



Examples of various recreational features within the proposed parks

should be consistent with neighbourhood themes (i.e. surrounding architectural styles and gateways).

- Lighting shall be provided for facilities and pathways, as required.
- Planting (trees, shrubs, grasses, perennials) shall comprise species tolerant of urban conditions with an emphasis on native species.
- Tree planting may reflect largely an informal layout with some more formal groupings of trees contained within lawn areas to facilitate shaded passive use.
- Potential features may include junior and senior play structures, multi-use trails, multi-purpose play courts, splash-pad, skateboard park, shade structure and seating, formal entries and seating, unprogrammed open space, structured sports fields, parking and site furnishings (i.e. trash receptacles, bicycle racks and lighting)
- Pedestrian circulation through the park should be clearly defined to encourage and promote public use. The walkway system within the park shall recognize connections to the surrounding street and sidewalk pattern and will align with connections to the trail system.
- Incorporate CPTED design principles of access control, territorial definition and natural surveillance, into site plan and landscape design. Landscaping and fence treatments should be designed to maximize natural surveillance of the park while demarcating the interface between public and private space.
- Housing that backs or flanks onto the parks shall be designed with enhanced rear and/or side elevation treatments (i.e. additional glazing, upgraded architectural detailing, roof form articulation, etc.).
- Park design details will be provided on the Landscape Plans to be reviewed and approved by the Town of Bradford West Gwillimbury.



## 6.5 Streetscape Design

The streetscape consists of the elements within the street right-of-way and of the built form located within the adjacent private realm which provides enclosure to the street zone. The streetscape plays a key role in promoting and enhancing the identity of a community and functioning as a common space for social interaction. A carefully considered combination of elements within the right-of-way can create an inviting and unique public realm experience. The design of streetscape elements shall be coordinated and consistent with the vision established for the Bradford Highlands subdivision in order to reinforce the character and identity of the community and ensure the safety, comfort and accessibility of pedestrians, cyclists and motorists.

The streetscape design elements within the Bradford Highlands neighbourhood will consist of:

- Street Trees
- Street Lighting
- Fencing
- Crosswalks
- Community Mailboxes
- Street Furniture
- Utilities

### 6.5.1 Street Trees

Street trees provide shade, reinforce view corridors and define the character of the streets. The following guidelines should be applied to the design of the Streetscape Plan:

- Boulevard trees will be located throughout the development to provide a sustainable amenity that will enhance the streetscape, provide visual interest, create shade to reduce the heat-island effect, and provide pedestrian comfort along sidewalks and trails.
- All proposed boulevard trees should be located to accommodate the canopies where driveways, swales and utilities will allow. All tree planting locations should be coordinated with the underground and above-ground utilities.
- Unless otherwise stipulated, the tree will be planted on the municipal side of the streetline. Spacing should be based on the municipal standards to create a continuous tree canopy at maturity. Where space is unrestricted, as along parks or storm pond frontages, trees are to be planted as per Town requirements and as determined by the landscape architect.
- At corner lots, there should be at least two trees planted along the longest edge in addition to the tree planted in front of the house. There should be no trees located within the sight-triangles.
- Ornamental or flowering trees shall be considered for key entry streets. Shrub and perennial planting may also be located in these areas to reinforce the community identity.



Conceptual images of street trees

- All tree planting locations should be coordinated with the underground and above-ground utilities. Trees should not be planted where there is a conflict with light poles, transformers, cable/telephone boxes or driveways.
- Street trees are to be attractive, high-branching deciduous trees that will help define the street edge and contribute to the pedestrian-oriented goals of the neighbourhood.
- Based on the Engineering Criteria Manual by the Town of Bradford West Gwillimbury, the following street trees are acceptable for the development:
  - Black Locust (*Robinia pseudoacacia*)
  - Silver Maple (*Acer saccharinum*)
  - Red Maple (*Acer rubrum*)
  - Sugar Maple (*Acer saccharum*)
  - Burr Oak (*Quercus macrocarpa*)
  - Red Oak (*Quercus rubra*)
  - Basswood (*Tilia americana*)
  - Serviceberry (*Amelanchier*)
  - Ironwood (*Ostrya virginiana*)
  - Hackberry (*Celtis occidentalis*)
  - White Elm (*Ulmus americana*)
  - Largetooth Aspen (*Populus grandidentata*)
  - Scarlet Oak (*Quercus coccinea*)
  - White Oak (*Quercus alba*)
  - Pin Oak (*Quercus palustris*)
  - Tulip Tree (*Liriodendron*)
- Trees of the same species should be planted on both sides of the street and may extend the length of the block or street, with the objective of creating a uniform canopy.
- To foster greater biodiversity, avoid street tree monocultures that repeat the same species over large areas.
- The trees should have a minimum caliper and height as per Town requirements, as specified by the Landscape Architect.
- Street tree planting to be completed per Town requirements.

## 6.5.2 Street Lighting

- Street lighting is an essential component of streetscape design that plays a key role in establishing the character of the public realm. High quality street lighting will



Example of street lighting

be located strategically throughout the site to ensure nighttime safety, security and enjoyment while preserving the ambiance of the night.

- Lighting design (pole and luminaire) should be selected based on aesthetics, maintenance, cost effectiveness and energy efficiency.
- Lighting design should be coordinated to promote a consistent and definable character for the community.
- Consideration should be given to establishing a hierarchy of coordinated light standards which are sized according to use related to vehicular routes, parking areas, walkway blocks and open space amenities, as appropriate.
- Pedestrian routes shall be well-lit to promote pedestrian safety and use of public spaces.
- Outdoor site and building lighting should be task oriented and not excessive.
- Use of full cut-off light fixtures that cast little or no light upward in public areas is recommended. All lighting should be 'night sky' compliant.
- Energy efficient LED lighting should be utilized to conserve resources. Opportunities should be considered for renewable energy use, such as solar-powered lighting.
- Selection and placement of lighting fixtures shall be in compliance with established Town of Bradford West Gwillimbury standards and local hydro authority requirements.



### 6.5.3 Fencing

Several types of fencing will be provided throughout the development, depending on the need for privacy and containment. In areas of high visibility, fencing shall be designed to enhance the streetscape appearance. The design of fencing visible from the public realm should portray a consistent theme through design, materials and colour to reinforce the character and identity of the community. All fencing shall be designed and installed in compliance with municipal standards and all applicable noise attenuation fencing requirements.

#### Wood Privacy Fence

- Corner lot fencing, where required by the Town, is intended to screen and enclose private low-rise residential rear yards otherwise exposed to flanking streets.
- Corner lot fencing shall be located within private property and follow the flankage lot line to a point near the rear corner (so that the side facade of the dwelling is not hidden from public view). The exact location of the fence will also be determined by the location of windows.
- This fencing shall return to within 1.2m of the flanking building face to accommodate a gate.
- All fences should have the same design and be the same colour.
- All fencing shall comply with the municipal standards.

#### Decorative Fencing

- Low decorative metal or wood fencing, accented at intervals with masonry pillars intermitted masonry columns, may be used in certain areas of the neighbourhoods to highlight the importance of public areas.
- Decorative fencing should be 0.9m to 1.2m in height.

#### Chainlink Fence

- Black vinyl chainlink fence is required where proposed residential lots abut open space features.

#### Noise Attenuation Fence

- Noise attenuation fencing may be required for certain dwellings within the neighbourhood in accordance with the applicable Noise Report.
- Fencing design, materials and heights shall comply with the requirements of the applicable Noise Report.



Wood privacy or noise attenuation fencing



Decorative fencing



Chainlink fencing



## 6.5.4 Crosswalks

Crosswalks provide a traffic calming feature that demarcates the route for pedestrians to safely cross the street thereby promoting active transportation and creating a pedestrian-friendly environment.

- In high pedestrian traffic areas a formal pedestrian crosswalk installation shall be provided at every four-way intersection.
- Pedestrian crosswalks shall be highly visible to motorists and include signage where appropriate.
- Intersections shall be designed and constructed in accordance with the Accessibility for Ontarians with Disabilities Act (AODA) and may include raised tactile surfaces and accessible pedestrian signals where appropriate.
- Curb ramps should be provided to facilitate wheel-chair and stroller usage in areas of high pedestrian activity, as per accessibility requirements. To assist pedestrians with visual impairments, curb ramp designs shall have raised tactile surfaces or materials with contrasting texture and sound properties.
- To enhance visibility and minimize conflicts between pedestrians and motorists, crosswalks at key intersections should consider enhanced intersections with distinctive coloured and/or textured materials or markings. This may include either zebra stripes (using retroreflective thermoplastic markings), broom finished concrete, concrete unit pavers, impressed concrete or an upgraded impressed asphalt (such as Streetprint XD).

- Design, siting and location of community mailboxes shall be in accordance with the requirements of both Canada Post and the Town of Bradford West Gwillimbury.



Conceptual examples of crosswalks

## 6.5.5 Community Mailboxes

- Community mailboxes will be located in easily accessible and highly visible locations in the community within walking distance for all residents.
- Community mailboxes are typically located in boulevards along flankage yards or in other centralized areas close to neighbourhood activity nodes. Mailboxes may also be integrated into stormwater management pond in close proximity to sidewalk and street.
- Community mailboxes provide opportunities to integrate attractive streetscape features as focal points within neighbourhoods where social interaction may occur.
- Mailboxes shall be located on a level paved surface in accordance with Canada Post's requirements.



Example of community mailbox within the streetscape



## 6.5.6 Street Furniture

Street furniture occurs within the public right-of-way and typically includes transit shelters, seating/benches, waste receptacles, public signage / sign blades, bicycle racks, rings or posts, etc. Attractive, sturdy, and accessible street furniture is fundamental to the visual appeal and use of streets and public spaces.

- Street furniture will be provided for the safety and convenience of users in high pedestrian traffic areas within key open space areas, such as parks and stormwater management pond lookouts, if proposed.
- The colour, material, form, and style of street furniture shall be consistent with and complementary to the established design theme for the community.
- The placement and layout of furnishings shall encourage safe use, maintain all accessibility requirements and be appropriate to the adjacent built form type and function.
- As much as possible, furnishings shall be vandal-resistant and low-maintenance, with readily available components.
- Wayfinding elements may be incorporated to provide clear and concise direction to users as well as providing community character in accordance with the Town of Bradford West Gwillimbury.



Images of potential street furniture

## 6.5.7 Utilities

- Above ground utility infrastructure should be located away from highly visible locations within the streetscape and designed to minimize their visual impact on the public and private realm, where feasible.
- Utility boxes should not be located along the frontage of parks and open space blocks, where feasible.
- Utility companies are encouraged to incorporate graffiti maintenance controls for applicable utility boxes.



Example of grouped utilities within the streetscape



## BUILT FORM GUIDELINES



A variety of well-designed built form will promote an attractive, pedestrian-friendly and sustainable community

## 7.1 Design Principles

Built Form Guidelines provide direction to establish architectural design criteria to assist in creating an attractive neighbourhood image. The design of built form within the Bradford Highlands subdivision should:

- Establish a high quality architectural character through development of appropriate architectural design standards.
- Create safe, pedestrian-friendly and attractive streetscapes, that promote a positive sense of place through building designs that provide a strong public face.
- Ensure that buildings on priority lots (i.e. corner lots, view terminus lots, lots adjacent to public open space areas) are given special design consideration to appropriately address a higher degree of public visibility.
- Promote variety and choice of residential building types, sizes and styles that help to establish identifiable character areas within the neighbourhood.
- Promote dwelling designs that minimize the visual impact of the garage.
- Promote architectural variety and innovation through flexible and adaptable guidelines.
- Ensure context sensitive buildings are designed to respond to their location in the neighbourhood and to adjoining uses.
- Provide for a high degree of environmental sustainability.

All new residential buildings within the neighbourhood will be subject to a privately administered architectural control process. A site plan approval process administered by the Town of Bradford West Gwillimbury will apply to non-residential development proposals (i.e. pumping station).



## 7.2 Building and Street Relationship

Attractive streetscapes typically consist of a landscaped (sodded and treed) boulevard adjacent to a defining edge of private front yards and carefully placed, well-designed dwellings. The following design guidelines should apply:

- The front façade of the dwelling should directly relate to the street.
- Building setbacks should define the street edge and create a visually ordered streetscape.
- Buildings should be designed to create harmonious massing within the streetscape.
- Primary building entrances should be clearly visible and identifiable from the street. Elevated entries and porches should be avoided. Ground related entries are preferred to minimize the negative visual impact of large concentrations of stairs.
- Projection into the front or flankage yard by porches, porticos and/or bays are encouraged for their beneficial impact on the streetscape.
- Corner buildings should be designed to address both street frontages in an equally enhanced manner.
- Buildings located at a view terminus should have an enhanced design to promote visual interest.
- Street-facing garages shall be subordinate to the habitable portion of the dwelling. Variety of garage configurations should be explored according to lot size.
- Privacy fencing should be provided for all corner dwellings and should not extend beyond the rear corner of the dwelling more than approximately 1.2m.



Buildings should be designed and sited to relate positively with the street



## 7.3 Architectural Character

- Architectural expressions that is defined by a blend of traditional-inspired and contemporary/ modern inspired homes will be a common theme that applies throughout the neighbourhood.
- A key consideration is to establish a palette of architectural styles that will complement the landscape design of the public realm to create an attractive neighbourhood image.
- A range of architectural styles should be explored and designed to diversify the neighbourhood and to ensure visual interest within the streetscape.
- Architecture should suit the building's use and location within the neighbourhood and exhibit a high quality character.
- Uninteresting generic architecture, devoid of character, will not be permitted.
- Refer to sample elevations on the following page.



Architectural styles are encouraged to vary to further characterize and give unique identity to the neighbourhood





Typical single detached dwellings



Typical semi-detached dwellings



Typical street townhouse dwellings



Typical back-to-back townhouse dwellings

Figure 7.3: Sample elevations prepared for Bayview Wellington Homes (prepared by VA3 Design)



## 7.4 High Quality Buildings

- A strong and sustainable neighbourhood character will be created with high quality architectural design.
- A rich vocabulary of architectural details, materials and colours consistent with the architectural style of the unit should be provided.
- Regardless of style, attention to high quality architectural detailing and materials is paramount.
- Use of high quality, low maintenance materials in a wide range of harmonious colours is required to ensure coordinated and attractive streetscapes.
- Durable building materials, such as brick, stone, stucco, and siding should be selected to support the intended architectural character of the building.



Examples of traditional building elements

Examples of contemporary building elements



## 7.5 Residential Built Form Typologies

A mix of built form typologies will be distributed throughout the Bradford Highlands subdivision and will include:

- Single detached dwellings on lot frontages 11.6m (38ft);
- Semi-detached dwellings on 7.6m (25ft) lot frontages;
- Street townhouse dwellings on 6.1m (20ft) lot frontages; and,
- Back-to-back townhouse dwellings on 6.4m (21ft) lot frontages.

The following general design criteria should be observed:

- A mix of lot sizes and variety of housing designs should be provided within each neighbourhood.
- Allow for a variety of architectural expressions and elevation treatment to provide visual diversity within the streetscape.
- All visible building facades should be well articulated and detailed. Design emphasis for buildings at focal locations will be required.
- The scale, height and massing of new housing should relate to the adjacent street while retaining a comfortable pedestrian scale.
- The visual impact of street-facing garages should be minimized.
- Provide ample fenestration and usable front porches to promote casual surveillance of public spaces from within the dwelling. This will contribute to a safe, pedestrian-friendly neighbourhood.



Single detached dwellings



Semi-detached dwellings



Street townhouse dwellings



Back-to-back townhouse dwellings

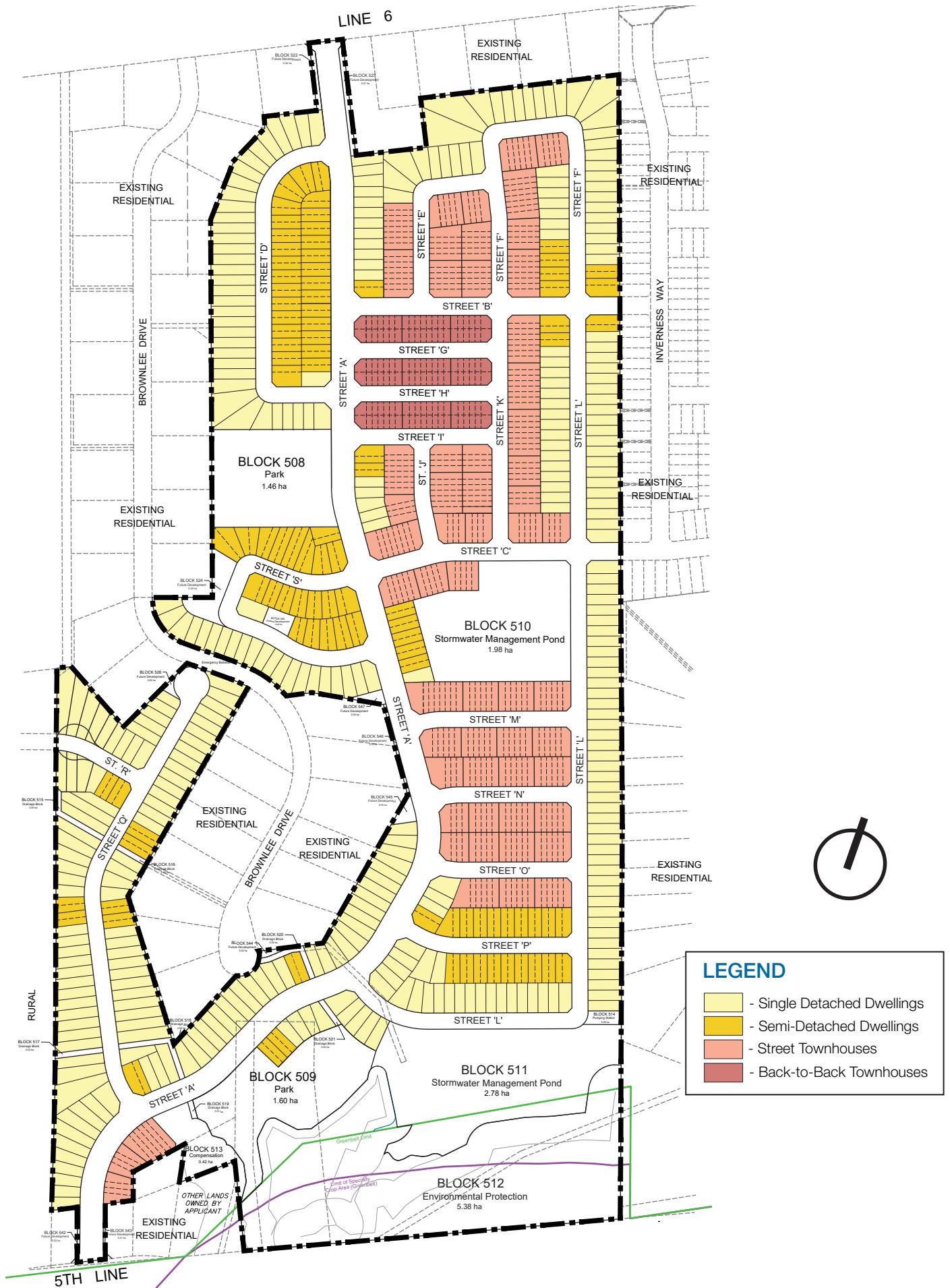


Figure 3.2: Teston Road Landowners Group Inc. Structuring Elements Plan



## 7.5.1 Single Detached Dwellings

Single detached dwellings will occur on lot frontages of 11.6m (38ft). The following design guidelines should apply:

- Single detached dwellings shall be designed to individually and collectively contribute to the character of the neighbourhood.
- Building elevations visible from public areas should incorporate appropriate massing, proportions, wall openings and plane variation in order to avoid large, uninteresting façades.
- A variety of bungalow, two storey, and three storey building massing will be permitted. It is important to ensure that appropriate measures are taken in the siting of dwellings to ensure compatible and harmonious massing relationships are achieved.
- For corner units, both street facing elevations shall be given a similar level of architectural treatment. Main entries for these dwellings are encouraged to be oriented to the flanking lot line.
- Garages shall be set behind or flush with the main building face or front porch.
- Attached street-facing garages should be incorporated into the main massing of the building to ensure they do not become a dominant element within the streetscape.
- All single detached dwellings may have double-car garages. Where double-car garages are provided, the preference is to have 2 individual garage doors separated by a masonry pier, where feasible.

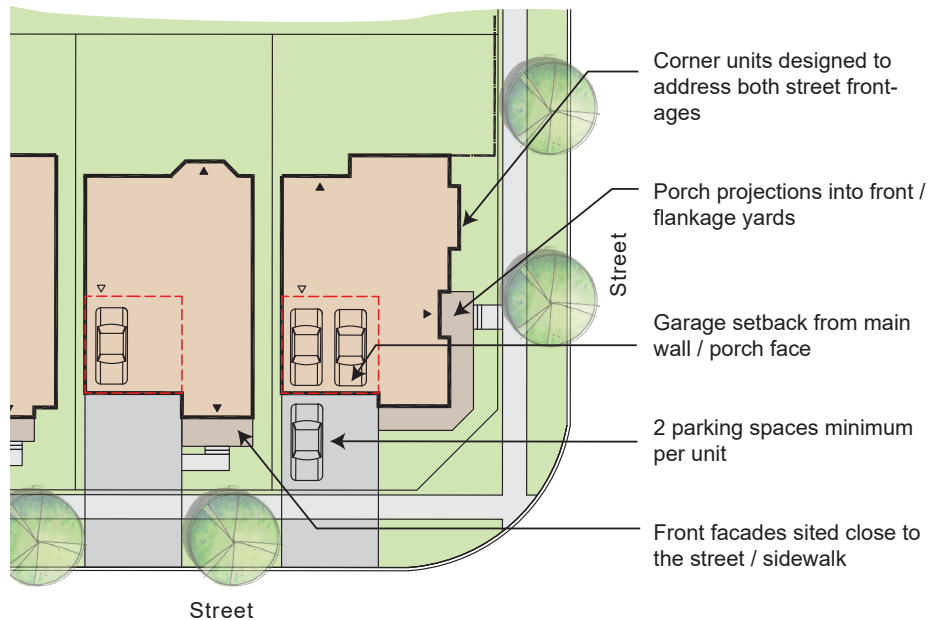


Figure 7.5.1: Conceptual plan layout for single detached dwellings



Conceptual images of single detached dwellings



## 7.5.2 Semi-Detached Dwellings

Semi-detached dwellings will occur on lots with frontages of 7.6m (25ft) or 15.2m (50ft) per pair. Semis contribute to the mix of housing types in the development and add to the diversity of housing choice and streetscape character. The following design guidelines should apply:

- The use of symmetrical and asymmetrical elevations are encouraged to generate streetscape massing variety. Both halves of the building shall be compatible in terms of design expression.
- Building elevations visible from public areas shall incorporate appropriate massing, proportions, wall openings and plane variation in order to avoid large, uninteresting façades.
- Each dwelling should have appropriate façade detailing, materials and colours consistent with its architectural style.
- Semi-detached dwellings should have 2- to 3-storey massing. Bungalows are generally discouraged for this housing type unless lofts are used to provide fenestration facing the street.
- Dwelling designs with covered front porches or porticos are encouraged, where appropriate to the architectural style.
- For corner lot buildings, the entry of the interior unit should be oriented to the front lot line, while the entry of the corner unit is encouraged to be oriented to the flanking lot line.
- Attached street-facing garages should be incorporated into the main massing of the building to ensure they do not dominate the streetscape.
- Semi-detached dwellings will have single-car attached garages with an additional parking space on the driveway.
- Garages / driveways for semi-detached dwellings should be paired to maximize on-street parking opportunities.

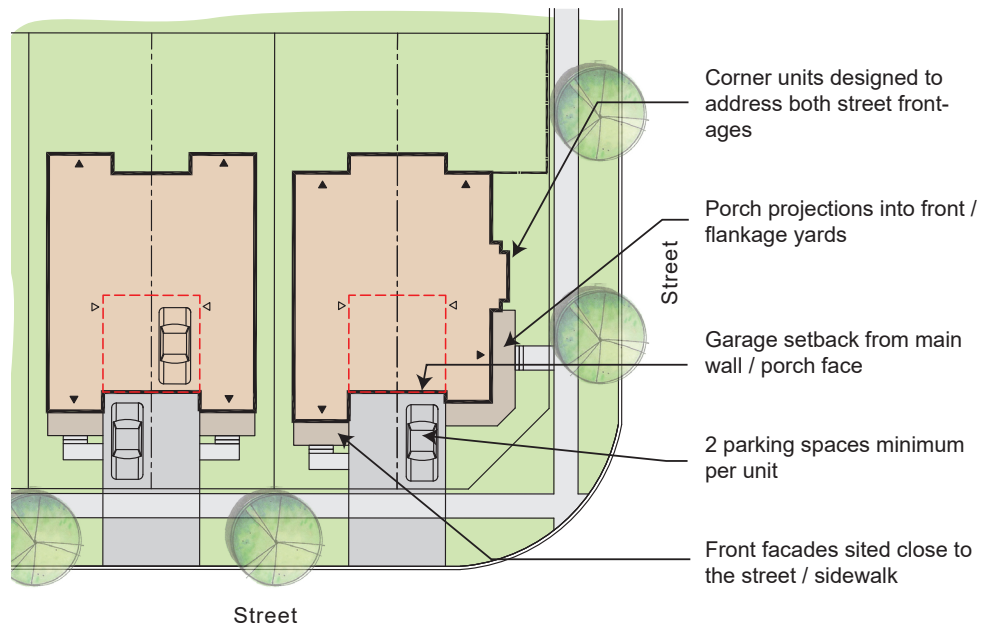


Figure 7.5.2: Conceptual plan layout for semi-detached dwellings



Conceptual images of semi-detached dwellings



### 7.5.3 Street Townhouse Dwellings

Street townhouse dwellings will occur throughout the development and are generally located on the east side of Street 'A'. Street townhouses will occur on minimum lot frontages of 6.1m (20ft), and will add built form diversity to the neighbourhood. The following design guidelines should apply:

- Townhouse blocks may be comprised of 4 to 8 units.
- The overall townhouse block composition should display massing and design continuity.
- Sufficient wall articulation is required to avoid large unbroken expanses wall planes, including the stepping of units and the use of bays, gables and porches where appropriate.
- Building elevations visible from public areas should incorporate appropriate massing, proportions, wall openings and plane variation in order to avoid large, uninteresting façades.
- Townhouse dwellings should have 2- to 3-storey massing. Bungalow forms are discouraged for this housing type.
- For corner lot buildings, the entry of the interior units shall be oriented to the front lot line, while the entry of the corner unit is encouraged to be oriented to the flanking lot line.
- Front-facing garages should be incorporated into the main massing of the building to ensure they do not become a dominant element within the streetscape.
- Townhouse dwellings will have single-car attached garages accessed from the street.
- Garages shall be set behind or flush with the main building face or front porch.
- Garages / driveways for townhouse dwellings should be paired, wherever feasible, to maximize on-street parking opportunities.
- Utility meters and air conditioning units shall be architecturally screened or discreetly located away from public view.

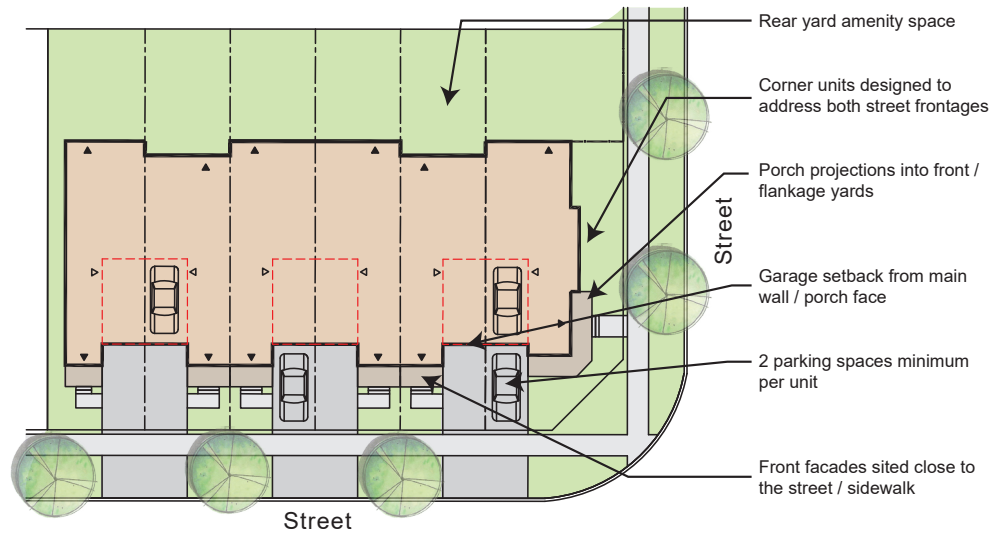


Figure 7.5.3: Conceptual plan layout for street townhouses



Conceptual images of traditional and contemporary inspired street townhouse dwellings



## 7.5.4 Back-to-Back Townhouse Dwellings

Back-to-Back Townhouses are a three storey housing form with front facing garages accessed from a public road, and occur on minimum lot frontages of 6.4m (21ft). As the name suggests there is a common demising wall along the rear of the unit in addition to the traditional interior side walls. Outdoor amenity space is provided in the form of a balcony typically located above the garage. In addition to the requirements stated in Section 7.5.3 the following design guidelines should apply:

- Each unit design shall provide an appropriate area for outdoor amenity space. Private outdoor amenity space is typically provided in the form of a balcony. Privacy screens should be provided between outdoor amenity spaces of neighbouring units.
- Since balconies will be facing the street, they must be well-detailed to suit the architectural style of the building using upgraded, durable and low maintenance materials.
- Dwellings should typically be sited in close relation to the street with minimal setbacks, wherever feasible. Reduced setbacks may be considered within mixed-use areas.
- Back-to-back townhouse dwellings will have single-car attached garages accessed from the street.
- Garages shall not project beyond the front wall or porch face of the dwelling.
- Garages will generally be paired.
- Entrances to each unit should be ground-related requiring no more than a few stairs to access, subject to site grading conditions.
- Utility meters and air conditioning units shall be architecturally screened or discreetly located away from public view.

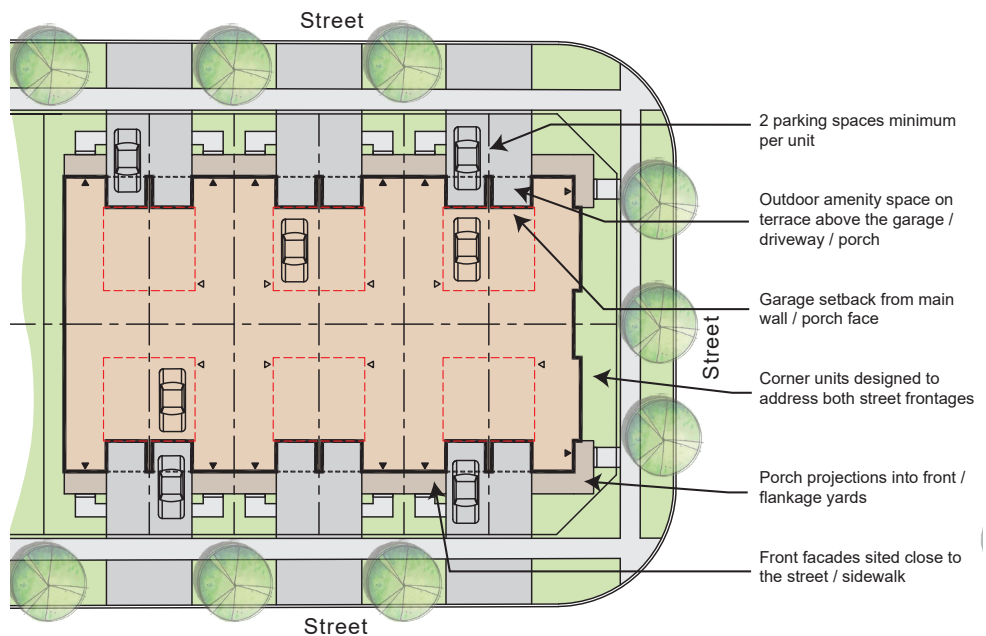


Figure 7.5.4: Conceptual plan layout for back-to-back townhouses



Conceptual images of back-to-back townhouse dwellings





Housing in visually prominent areas within the Bradford Highlands subdivision will be deemed priority lot dwellings

## 7.6 Priority Lot Dwellings

Priority Lot Dwellings are located prominently within the neighbourhood. Their visual significance within the streetscape demand that the siting, architecture and landscaping of buildings on these lots be of an exemplary quality to act as landmarks within the neighbourhood. Special architectural design consideration is required for the publicly exposed elevations of these dwellings to promote strong visual interest and a sense of place.

Priority Lot Dwellings occur at:

- Corner lots;
- View terminus lots;
- Dwellings requiring upgraded rear and/ or side architecture; and,
- Parking facing dwellings.





**LEGEND**

- - Corner Dwellings
- T - View Terminus Dwellings
- ■ ■ - Upgraded Rear and/ or Side Architecture
- ■ ■ - Park Facing Dwellings

NOTE: Where rear elevations are obscured by mature vegetation not readily visible to public areas, or adjacent to open spaces with no public access, rear elevation upgrading will not be required.

Figure 7.6: Bradford Highlands Joint Venture Priority Lot Map



## 7.6.1 Corner Lot Dwellings

Dwellings on corner lots are very prominent within the streetscape and will require special designs which address the flanking elevation in a manner consistent with the front elevation.

- Porches, porticos, bay windows, generous fenestration, wall articulation and/or other interesting architectural features are required on the flanking side to create a positive pedestrian presence along the street.
- The preferred design is with the main entrance facing the long or flanking side of the lot. This treatment shall be provided on the majority of corner units sited.
- Highly articulated flanking elevations are required to avoid flat, blank, uninteresting façades.
- Rear yard privacy fencing is required and shall be placed in such a manner so it does not hide the flanking building elevation.

## 7.6.2 View Terminus Dwellings

- View Terminus Dwellings occur at the top of 'T' intersections and at street elbows.
- These dwellings play an important role in the streetscape by terminating a long view corridor.
- Because of their prominence, View Terminus Dwellings should include architectural and features and landscaping elements that serve to enhance the view corridor and provide visual interest.
- Dwellings on the outside bend of curved streets provide opportunities to create a grouping of dwellings that accentuate a special architectural and landscape theme.
- Where dwellings step back from each other, publicly exposed side walls should be given similar architectural detailing as the front elevation.



Examples of corner lot architecture



Example of view terminus dwellings



### 7.6.3 Upgraded Rear and Side Architecture

- Upgraded Rear and Side Architecture is required where these elevations are exposed to public view. This will be required for dwellings which back or flank onto roads, parks, and public open space areas.
- The exposed side and/or rear elevations of these dwellings shall have a level of quality and detail consistent with the front elevation of the dwelling.
- The level of upgrading should be consistent with the level of public exposure. For example, houses backing onto a park will be quite visible to an open area of frequent public use and will require a higher level of upgrading than dwellings backing onto densely treed open space areas.



Example of side upgrade architecture

### 7.6.4 Park Facing Dwellings

- Park facing dwellings are very visible within the public realm and shall have a high degree of architectural detailing consistent with the architectural style of the dwelling, such as large, well proportioned windows, a projecting bay, or other design feature to reflect their visual prominence.
- The use of upgraded building materials, such as stone or precast detailing is encouraged to reflect the upscale nature of the community.
- Dwellings are encouraged to have wider and deeper porches which will promote 'eyes on the street' and will provide for an added safety feature and increase social interaction among neighbours.



Example of rear upgrade architecture



Example of park facing dwellings



# NON-RESIDENTIAL BUILT FORM GUIDELINES

## 8.1 Utility Buildings

A utility building will be required within the Bradford Highlands subdivision for such purposes as sewage pumping, telecommunications, hydro, etc., and should be designed in accordance with the following objectives:

- Utility Buildings are to be located discretely within the community where they will not be highly visible. Where possible, they should be located within a landscaped area in close proximity to, or within, a stormwater management facility block. Locations shall be determined during the block planning process.
- When located in an open space area or SWM block, the Utility Building should be treated as a feature and given architectural design emphasis.
- The appearance of Utility Buildings should be integral to the overall streetscape.
- The exterior appearance of Utility Buildings shall exhibit residential design characteristics.
- The use of pitched roofs, articulated street-facing walls and exterior materials which are harmonious with the residential architecture of the community (i.e. brick and/or stone).
- Associated air condition units and/or mechanical equipment are to be oriented as far away as possible from adjacent residential areas.
- Integrated landscaping should be provided around the Utility Building.



Conceptual images of utility buildings

# LOW IMPACT DEVELOPMENT AND SUSTAINABILITY

Sustainability includes the interface of environmental, social, economic and cultural influences that ensure a community remains balanced and productive. Managing and protecting valuable resources through design and construction will result in the conservation of those resources in the overall lifespan of the community. The design objective is to create sustainable urban form that supports compact development, greater walkability and transit use, site and building adaptability, intensification versus sprawl, conservation of natural areas, building in harmony with the surrounding environment and a greater use of existing infrastructure.

Sustainable development will be promoted in Bradford Highlands subdivision in both the design of the subdivision and in new home construction in order to:

- Provide a high quality of life for residents.
- Be cost effective to build, operate and maintain.
- Accommodate growth through compact development on a street-grid road system supported by alternative transportation modes.
- Reinforce walkability / cycling.
- Be transit supportive.
- Minimize environmental impacts.
- Be resilient to climate/weather-related events.
- Promote water conservation and energy efficiency.



- Promote green building design.
- Provide for construction of buildings that consider both energy efficiency and conservation in order to enhance building performance, lower utility bills and result in greater environmental protection overall.
- Consider incorporating alternative energy sources.
- Combine living, working and playing environments in close proximity.

## 9.1 Development Considerations

The following sustainable development practices may be considered:

- Low Impact Development techniques on private property that encourage stormwater to be treated where it falls, thereby improving water quality and quantity on the site.
- Reduce impermeable surfaces and stormwater runoff (including bio-retention, drought tolerant vegetation, rain gardens, etc.).
- Mitigate stormwater flow through the integration of stormwater management ponds and drainage pools.
- Provide additional depth topsoil placement on lots.
- Provide landscaping that increases the urban tree canopy.
  - Provide natural feature and valley land restoration areas and edge management planting.
  - Provide LED street lighting.
  - Source local materials and manufactured components.
  - Pedestrian connectivity and links to transit stops to promote active transportation and transit usage.



- Design street and block alignments to maximize overall site passive solar gain – an east/west alignment typically serves this purpose.

## 9.2 Building Considerations

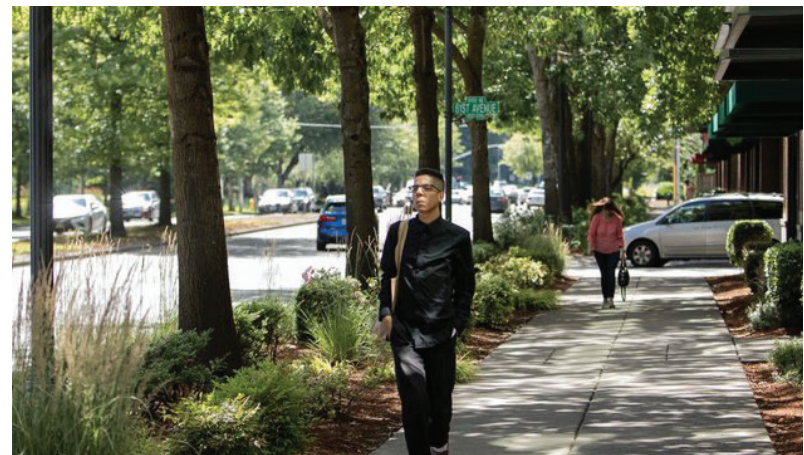
All new low-rise homes will be subject to the requirements of the Ontario Building Code which incorporates a range of energy efficient building standards. In addition to this, the following energy efficiency and conservation measures may be considered in the design and construction of new homes:

- Supply water efficient fixtures throughout the home.
- Energy efficient lighting fixtures and appliances.
- Occupancy sensors in main living areas and motion sensors for exterior lighting.
- Energy efficient heating, ventilation and cooling (HVAC) systems.
- The provision of a heat recovery ventilation system (HRV or ERV).
- Energy efficient windows/patio doors to help reduce the need for air conditioning in the summer and heating in the winter.
- Ensure the home is tightly sealed to reduce drafts.
- Utilize low-emitting adhesives and sealants, paints and coatings, and carpets and wood flooring.
- Employ a waste management policy to ensure that all trades work efficiently to reduce, eliminate or recycle waste.
- Provide and maintain erosion sediment control at all times during construction, in accordance with approved plans.
- Purchase stone, concrete and masonry from regional/local sources.
- Use low maintenance building materials.
- Use materials with recycled content.
- Electric vehicle charging stations are encouraged.

## 9.3 Walkability and Cycling

Promoting active transportation is a key urban design principle for the Bradford Highlands subdivision. A major factor in creating a sustainable and healthy community will be promoting pedestrian and cyclist connectivity, comfort and safety. Provision of public sidewalks, multi-use paths, and trails will offer pedestrians and cyclists alternatives to vehicular travel through the community. Key destinations, such as the various open space and institutional assets within the community have been located and designed within walking distance of the residential neighbourhoods. The following design guidelines should be considered:

- Attractive, safe and pedestrian-scaled environments shall be created to maximize pedestrian comfort.
- Sidewalk, multi-use path, and trail systems shall be interconnected and provide for ease of navigation.
- An inclusive walkable community shall be promoted to reduce barriers for persons with disabilities, seniors, strollers, etc.



Promoting pedestrian and cyclist connectivity, comfort and safety will assist in creating a sustainable and healthy community

# SECTION 10

## IMPLEMENTATION

### 10.1 Planning Approvals

The Bradford Highlands Joint Venture UDR provides the overall design direction for development of both the private and public realms within the community and will be implemented through:

#### 10.1.1 Draft Plan

- The Draft Plan approval process will establish the structuring elements for the subdivision, including general land use organization, road pattern, lot configuration, location of community elements, and pedestrian circulation.

#### 10.1.2 Zoning By-Law

- The implementing Zoning By-Law for the Bradford Highlands subdivision will be written based upon the Draft Plan of Subdivision having regard to the principles and recommendations established in the Bradford Highlands Joint Venture Urban Design Report.

#### 10.1.3 Subdivision Agreements

- The detailed subdivision design requirements and developer obligations are established within the Subdivision Agreement.
- This will include the requirements for provision of a detailed Master Landscape Plan and an Architectural Control Process administered privately on behalf of the Town.

### 10.2 Architectural Control

#### 10.2.1 Architectural Control Guidelines

- A requirement for detailed Architectural Control Guidelines and implementation of an Architectural Control Process will be required prior to any development.
- The Architectural Control Guidelines deal specifically with site planning and architectural matters on private lands and will provide further detail to the broad design principles established within the Bradford Highlands Joint Venture Urban Design Report.
- The Architectural Control Guidelines will be based on the approved Bradford Highlands Joint Venture Urban Design Report. The Architectural Control Guidelines shall also articulate how the designs and concepts are consistent with the performance standards outlined in the Town of Bradford West Gwillimbury Town-Wide Urban Design Guidelines and the Town's Official Plan.
- The Architectural Control Guidelines shall ensure conformance with Town-Wide Urban Design Guidelines, including all up-to-date construction details and standards.
- The Architectural Control Guidelines require approval by the Town of Bradford West Gwillimbury.

#### 10.2.2 Architectural Control Process

- The Architectural Control Process will be administered privately on behalf of the Town by a Control Architect satisfactory to the Town.
- The Architectural Control design review process will include the review and approval of all residential building plans and elevations, exterior materials and colours, fencing (where required to be installed by the Builder) and siting/grading plans for conformity with the Architectural Control Guidelines prior to the submission of plans to the municipality for building permit.



- All applications for a building permit shall be stamped by the Control Architect as conforming to the applicable guidelines.
- In no case shall the Control Architect and the design architect for the building be the same individual or firm.
- The Town retains the authority to require the appointment of a new Control Architect should the Town not be satisfied with the manner in which the Guidelines are being implemented.
- The Landscape Master Plan shall ensure that all proposed infrastructure and streetscaping within the road allowance is designed in conformance of standards and details set out by the Town of Bradford West Gwillimbury and the Town-wide Urban Design Guidelines.

### 10.4.2 Detailed Landscape Drawings

- Detailed landscape drawings shall be based on the approved Master Landscape Plan.
- This will be administered by the Town of Bradford West Gwillimbury.
- A Maintenance Matrix will be required upon approval of detailed subdivision design drawings. The matrix shall include maintenance and life cycles for all landscape related works, including potential phasing.

## 10.3 Site Plan Approval Process

- A Site Plan Approval process administered by the Town of Bradford West Gwillimbury will be required prior to development of non-residential uses (i.e. the pumping station) as deemed necessary by the Town of Bradford West Gwillimbury.
- This may be administered in conjunction with the Control Architect.
- The Site Plan Approval process will also require detailed landscape drawings.

## 10.4 Landscape Plans

### 10.4.1 Landscape Master Plan

- As a condition of Draft Approval a Landscape Master Plan will be required.
- The Landscape Master Plan will be based on the approved Bradford Highlands Joint Venture Urban Design Report. The Landscape Master Plan shall also articulate how the designs and concepts are consistent with the performance standards outlined in the Town of Bradford West Gwillimbury Town-Wide Urban Design Guidelines and the Town's Official Plan.
- Detailed design for all components of the subdivision shall be provided. This includes: streetscape design; fencing; parks; stormwater management ponds; walkways/trails.
- The Landscape Master Plan shall ensure conformance with Town of Bradford West Gwillimbury Town-Wide Urban Design Guidelines, including all up-to-date construction details and standards.